

# SHERWOOD VISION 2040







## Acknowledgements

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### City Council

Mayor Virginia Hillman Young  
April Broderick  
Charlie Harmon  
Mary Jo Heye  
Kevin Lilly  
Beverly Williams  
Marina Brooks  
Tim McMinn  
Mike Sanders  
Angela Nicholson, City Clerk

### Planning Commission

Rob Middleton, Chair  
Blake Martin  
Lucien Gillham  
Steve Bruno  
Danny Knight

### Steering Committee

Rob Middleton, Chair  
Mayor Virginia Young  
Beverly Williams  
Marina Brooks  
April Broderick  
Mike Sanders  
Kevin Lilly  
Steve Cobb  
Blake Martin  
Steve Bruno  
Jacob Hall  
Michelle Oglesby  
Nikolas Morgan  
Shelby Thomas  
Steve Deere  
Veronica Tice  
Stacey Galloway  
Colleen Carr  
Richard Penn  
Candi Ussery

Prepared with assistance from:





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## ONE: INTRODUCTION

This document contains the officially adopted Comprehensive Plan for Sherwood, Arkansas, known as Sherwood Vision 2040. In late 2017, the City of Sherwood began the process to create Sherwood Vision 2040. This effort was undertaken to create a cohesive, consensus community vision and providing a guide for future growth. Additionally, the city needed to determine a framework for future revisions to its land use regulations. By engaging residents, elected and appointed officials, business leaders, and others, the plan represents a united vision of Sherwood's future for the next 25 years. The Plan, which was completed in mid-2019, articulates and identifies how development, mobility, the environment, recreation, public services, and character all interrelate and will guide Sherwood's future.

*Sherwood Vision 2040 provides a framework for updating the city's outdated and confusing land use regulations.*

### ***What is the Comprehensive Plan?***

The plan serves as an official policy statement of the City of Sherwood for directing growth and development within its city limits and planning area as well as articulating strategies to address Sherwood's current and future challenges. A steering committee directed the preparation of the plan during a process that included careful study of the area. Areas of analysis include Sherwood's history, demographics and projected population, community and stakeholder opinions, topography, utility capacity, transportation systems, existing infrastructure, and land use among myriad other items.

*Sherwood Vision 2040 is an official policy statement of the City of Sherwood establishing a consensus vision to guide the community over the next 25 years.*

The plan will help guide the decisions of both the Planning Commission and City Council during the planning period, estimated to be 25 years. It is additionally intended to outline strategies and actions for key private entities as well, such as the Chamber of Commerce. The Comprehensive Plan serves all residents and property owners within the planning area as well as others having a stake in the future of the city.

The Comprehensive Plan provides a broad guideline for growth and development. It is not meant to direct land use arrangement precisely, nor is it a zoning ordinance. It serves as an instrument to blend public and private interests in a manner that will best suit the entire community. Citizens and business interests may look at the plan as a "constitution" for the City of Sherwood. Unlike a national constitution, it may change more often to meet new challenges and growth issues.

The plan will remain flexible, allowing for necessary modification of land uses. It plots land usage areas according to long-term community needs, not short-term individual gains. Planning should also be based on sound development principles. The plan addresses pertinent community issues as a whole rather than treating isolated problems as they may arise.



One of Sherwood's many new subdivisions.

## ***Planning Process***

The planning process consisted of three primary phases. The following details the work that was undertaken.



## ***Authority***

The purpose of the Comprehensive Plan is consistent with the provisions of Arkansas Codes, Annotated (A.C.A.), §14-56-403. This section requires that plans of a municipality be "... prepared in order to promote, in accordance with present and future needs, the safety, morals, order, convenience, and general welfare of the citizens." The statutes further state that plans may provide for, among other things, the following:

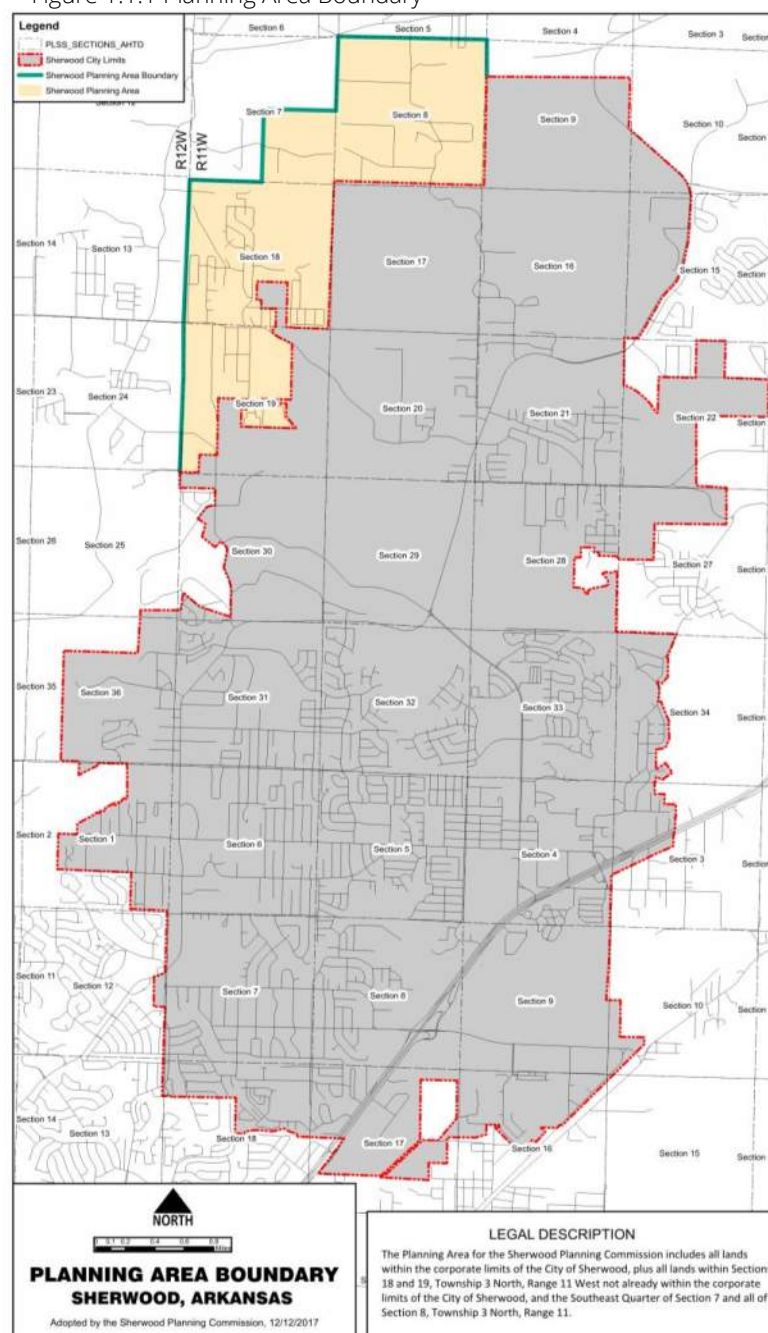
- *Efficiency and economy in the process of development*
- *The appropriate and best use of land*
- *Convenience of traffic and circulation of people and goods*
- *Safety from fire and other dangers*
- *Adequate light and air in the use and occupancy of buildings*
- *Healthful and convenient distribution of population*
- *Good civic design and arrangement*
- *Adequate public utilities and facilities*
- *Wise and efficient expenditure of funds*



## Planning Area

The Sherwood Planning Area Boundary appears in graphic form on the plan map, planning area boundary map, and other maps used with this document. The Planning Area Map was prepared in accordance with statutes found in the Arkansas Codes, Annotated § 14-56-413. A copy is on file with the City Clerk and the Pulaski County Recorder. The Planning Area Boundary depicted on the Comprehensive Plan map includes those lands within the territorial jurisdiction of Sherwood for which it may prepare plans, ordinances, and regulations. This area extends beyond the city limits to include those areas most likely to become a part of the city within a period of twenty-five years. The City of Sherwood will, in accordance with A.C.A. § 14-56-422, file the plans, ordinances, and regulations as they pertain to the territory beyond the corporate limits with the county recorder of Pulaski County.

Figure 1.1.1 Planning Area Boundary



### ***Relationship to the Land Use Regulations***

The Arkansas planning statutes, in A.C.A. § 14-56-416 (a)(1) provide:

*Following adoption and filing of the land use plan, the commission may prepare for submission to the legislative body a recommended zoning ordinance for the entire area of the municipality.*

The statutes further provide in A.C.A. § 14-56-417 (a)(1):

*Following adoption and filing of a master street plan, the Planning Commission may prepare and shall administer, after approval of the legislative body, regulations controlling the development of land.*

These provisions, along with the modern history of planning since the landmark case of Village of Euclid, Ohio v. Ambler Realty Co., 272 U.S. 365 (1926), signify a strong relationship between the plan and its supporting regulations. In simple terms, a municipality first plans and then regulates. The primary supporting regulations consist of the zoning code and development (subdivision) regulations. As stated in A.C.A. § 14-56-412 (e):

*In order to promote, regulate, and control development and to protect the various elements of the plans, the commission, after adoption of appropriate plans as provided, may prepare and transmit to the legislative body such ordinances and regulations as are deemed necessary to carry out the intent of the plans, or of parts thereof.*

Planners take these provisions literally and encourage municipalities to base decisions in land use and development upon adopted plans to the greatest extent possible. At the same time, it has been noted in court decisions in Arkansas that plans are not legal documents but rather broad statements of municipal policy. The legal force arises from the adopted regulations developed to support the plan. In order to reconcile these considerations, the Planning Commission will first determine if a proposal deviates from the spirit and intent of the plan. If it does, the Commission will then consider an amendment to the plan before considering the proposal.

## TWO: CONTEXT AND TRENDS

### 2.1: History and Regional Setting

#### History

Though a relatively young city, having been incorporated only in 1948, the community's area has a long history. In 1820, Benjamin Kellogg, namesake of Kellogg Creek, settled in the area now known as Sylvan Hills. By the 1840s, silver, lead, and zinc was discovered and being mined from Kellogg's land. The mines were an object of interest for both the Confederate and Union armies during the Civil War.

The area covering the original incorporated area of "Old Sherwood" was settled by Frank and Mary Calva in 1881. By the 1920s, Justin Matthews, founder of Metropolitan Realty & Development, acquired property in area and began development. Mr. Matthews and his company have been responsible for development of significant portions of both Sherwood and North Little Rock. Among those developments was the Sylvan Hills Country Club, serving as a catalyst for the Sylvan Hills Subdivision in "Old Sherwood". Development of the area was slowed by the Great Depression and World War II, which also delayed reconstruction of the Sylvan Hills clubhouse after it was destroyed by fire in 1935. However, by the time the city incorporated in the late 1940s, over 700 people lived in Sherwood.



Kellogg Mines, circa 1926.



An ad for Sylvan Hills CC, circa 1928.

The city's first business, Sherwood Grocery, opened in 1947, at 117 Country Club Road. Sherwood got its first post office in 1966, and its first bank in 1967. Plans for the first elementary school began in 1959, as the city had 388 children under the age of 12. The town raised money with a beauty contest. The school was supposed to open on September 6, 1960, but a sewer problem delayed the opening. Today the community is served by the Pulaski County Special School District with all the city's students attending Sylvan Hill High School.

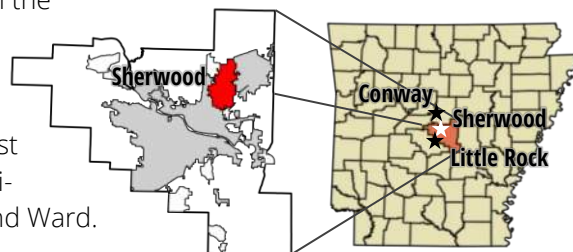
Over the years, Sherwood has steadily grown toward the north as Metropolitan Realty & Development, and other companies, have constructed new subdivisions and accompanying commercial development. Sherwood took a giant leap in 2008, when it jostled with the city of Jacksonville to annex the Gravel Ridge community. The contentious battle resulted in the size of the city nearly doubling overnight, leaving the community room to grow for decades to come.

Today, Sherwood is a burgeoning quality of life community and one of the fastest growing cities in the Central Arkansas region. Throughout its history, the city has devoted time, effort, resources to making Sherwood a place that values community and invests in itself by ensuring the community enjoys high-quality amenities. This legacy will help ensure Sherwood continues as a growing city in the future.

Adapted from *The Encyclopedia of Arkansas History & Culture*, *Darrel W. Brown's History of Sherwood*, and the *Arkansas Historic Preservation Program*.

## ***Regional Setting***

Sherwood, part of the Little Rock metropolitan area, is located in the northeast part of Arkansas' most populous county – Pulaski County. The community is within the Central Arkansas region, directly adjacent to North Little Rock, 11 miles northeast of Little Rock, 35 miles southeast of Conway, and 33 miles northeast of Benton. Other nearby communities forming the North Pulaski-North Lonoke County area include Jacksonville, Cabot, Austin, and Ward.



Sherwood has one major corridor running through it, U.S. Highway 67/167 - also known as Future Interstate 57. Future Interstate 57 is planned to connect to I-30 with an extension from Sikeston, Missouri through northeast Arkansas to North Little Rock. In its current form, U.S. Highway 67/167 is an already important federal highway for trade helping connect several large Texas metro areas – Dallas, Houston, San Antonio, and Austin to Chicago. As such, this highway helps connect three of the five largest metro areas in the United States. Other corridors in the city include Arkansas Highway 107, an important regional arterial connecting the north metro area, as well as Arkansas Highway 176, which serves as the city's main east-west corridor.

***Sherwood is located along a major U.S. trade corridor, Future Interstate 57, which connects three of the country's five largest metro areas.***

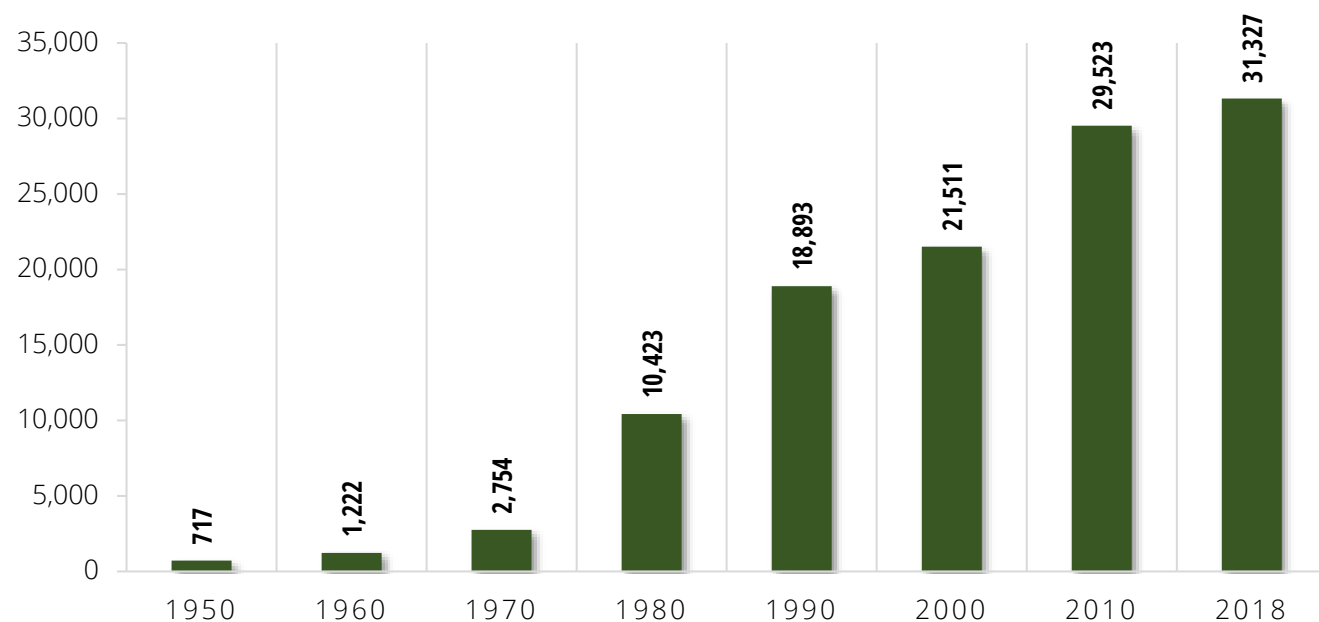
Sherwood is also connected to the rest of the country through nearby airports, railroads, and the Arkansas River. Commercial air service is available at Clinton National Airport in Little Rock with non-stop and daily service across the U.S. The city is also served by Class I rail through Union Pacific Railroad. In addition, the Arkansas River runs just to the south of the city

and is navigable by barge with major port facilities at the nearby Little Rock Port. Overall, the transportation system in Sherwood is exceptional and gives the city an intermodal advantage over other areas in the state.

## 2.2: Trends

### Population

Figure 2.2.1 Population Growth – Sherwood



Source: U.S. Census Bureau

Table 2.2.2 Selected Ethnic and Racial Characteristics – Sherwood and Selected Entities

Race	Sherwood	Benton	Arkansas
White Alone	73.2%	86.4%	77.3%
Black or African American Alone	21.0%	8.2%	15.4%
American Indian	0.4%	0.6%	0.7%
Asian Alone	1.6%	1.2%	1.4%
Native Hawaiian	0.0%	0.0%	0.2%
Some Other Race Alone	1.5%	1.2%	2.5%
Two or More races	2.2%	2.1%	2.4%
Hispanic	5.9%	6.0%	7.2%

Source: U.S. Census Bureau 2017

Figure 2.2.3 Age Distribution – Sherwood

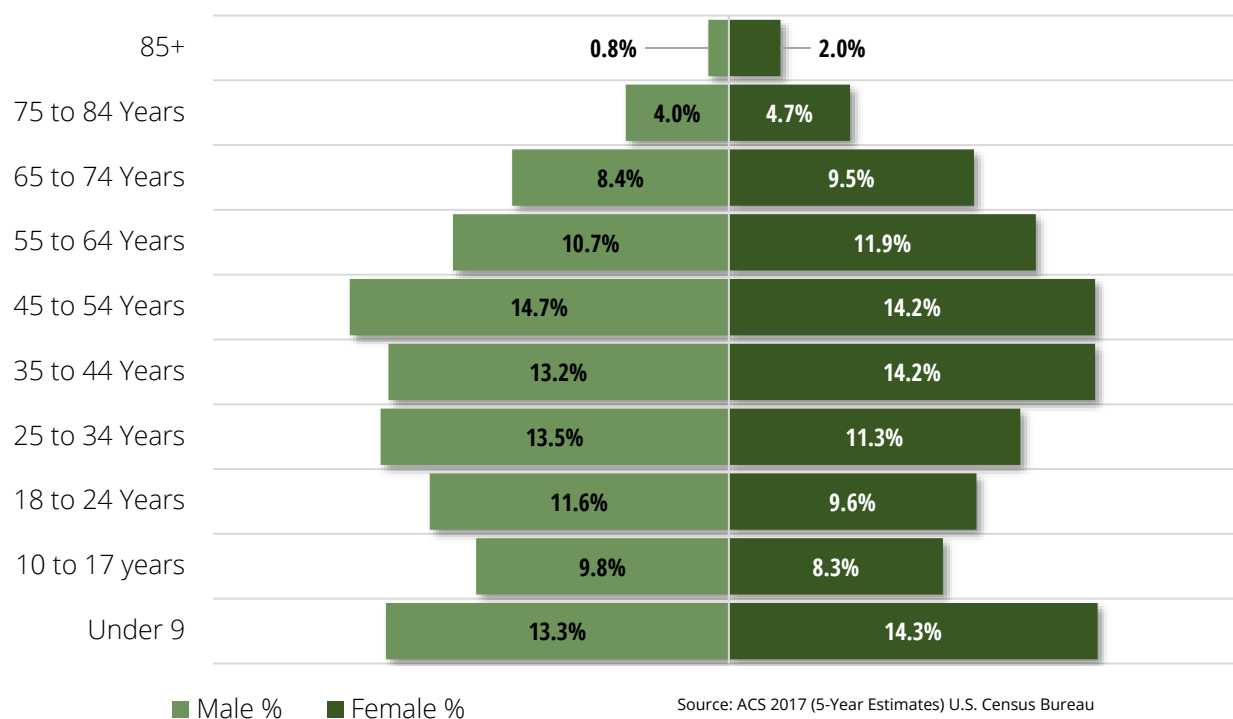
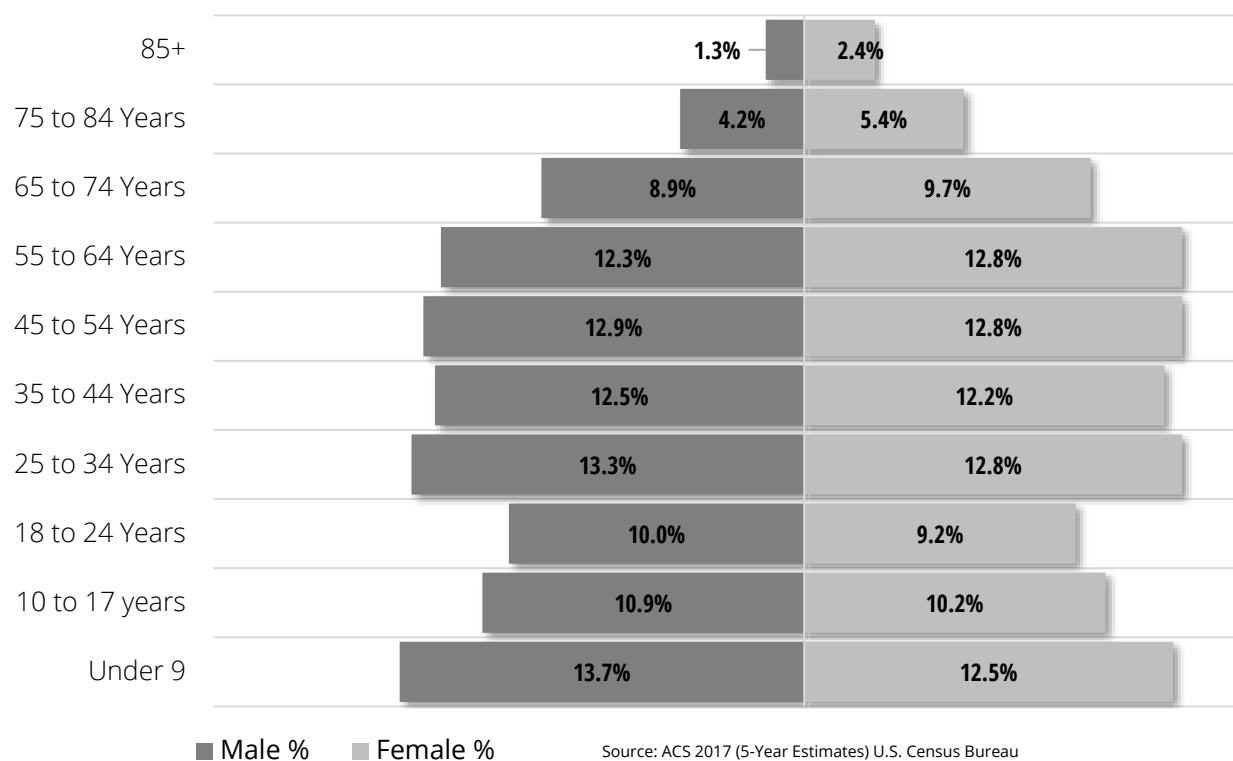


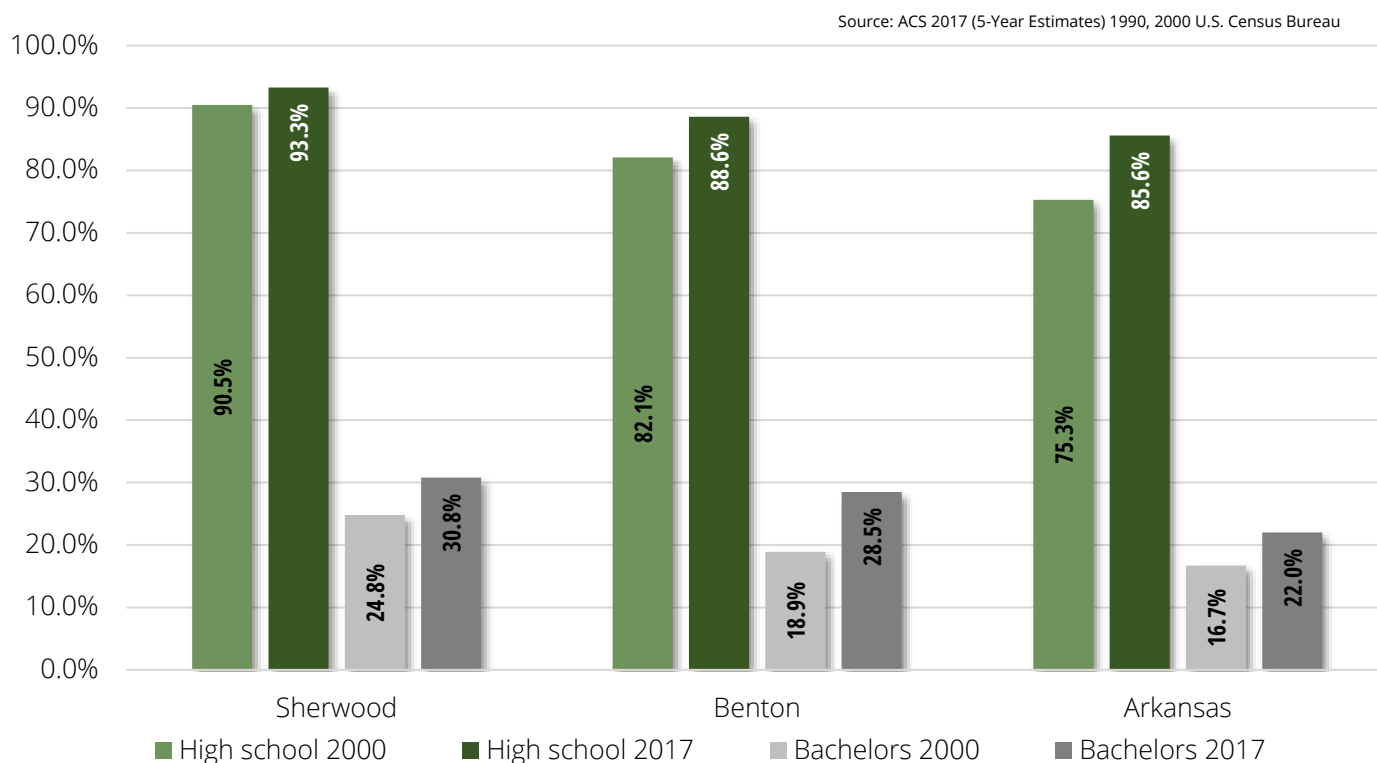
Figure 2.2.4 Age Distribution – Arkansas



- Sherwood has seen strong and steady population growth since the 1970s, fueled by the city's suburban lifestyle and proximity to downtown Little Rock employment.
- Since the Great Recession, the city's growth has slowed. However, construction trends appear to indicate a return to growth trends seen before the recession.
- Sherwood's African American population increased 194% between 2000 and 2017, accounting for approximately 45% of the city's growth in the same time period.
- Sherwood's Hispanic population increased 307% between 2000 and 2017, accounting for approximately 15% of the city's growth in the same time period.
- Sherwood's age distribution is very similar to that of entire state. While Sherwood has a slightly older population, the city has a very healthy population distribution that indicates it is an attractive place to live for all age groups.
- The city's age distribution indicates slow to little natural population increase, meaning that most of the city's population growth can be attributed to migration into the city.

## Education

Figure 2.2.5 Educational Attainment – Sherwood and Selected Entities

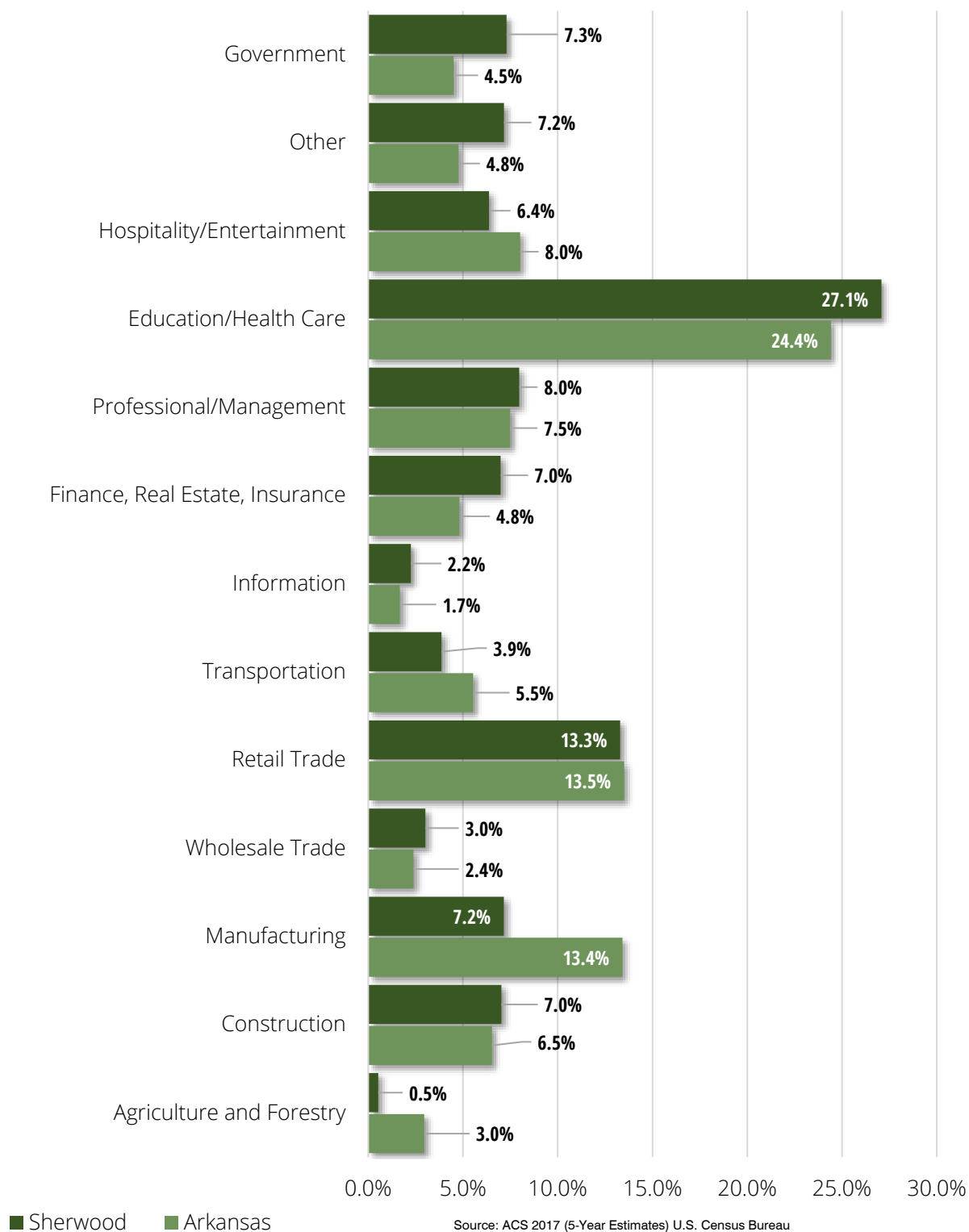


- Educational attainment is a strong point for the community. The city's share of residents with at least a bachelor's degree matches that of the nation as a whole and exceeds state and regional peers. The city's rate of attainment for high school equivalency exceeds the nation, state, and regional peers.
- Though not shown in a specific chart, Sherwood has a high share of its population possessing a graduate degree at 10.4%.



## Employment

Figure 2.2.6 Occupational Composition – Sherwood and Arkansas



- Sherwood residents are engaged in occupations that reflect a high degree of economic diversity. This is representative of the diversity of the metro area's economy as a whole.
- Sherwood residents are employed at higher rates than the state in occupations which earn higher incomes. Examples of this include finance, professional/management, information, and healthcare/education. A very low percentage of the city's population is employed in agriculture or manufacturing compared to the state as a whole.
- The indicated occupational composition indicates the relative economic health of Sherwood and the metro area.

## Income

Figure 2.2.7 Median Household Income – Sherwood and Selected Entities (2017 Dollars)

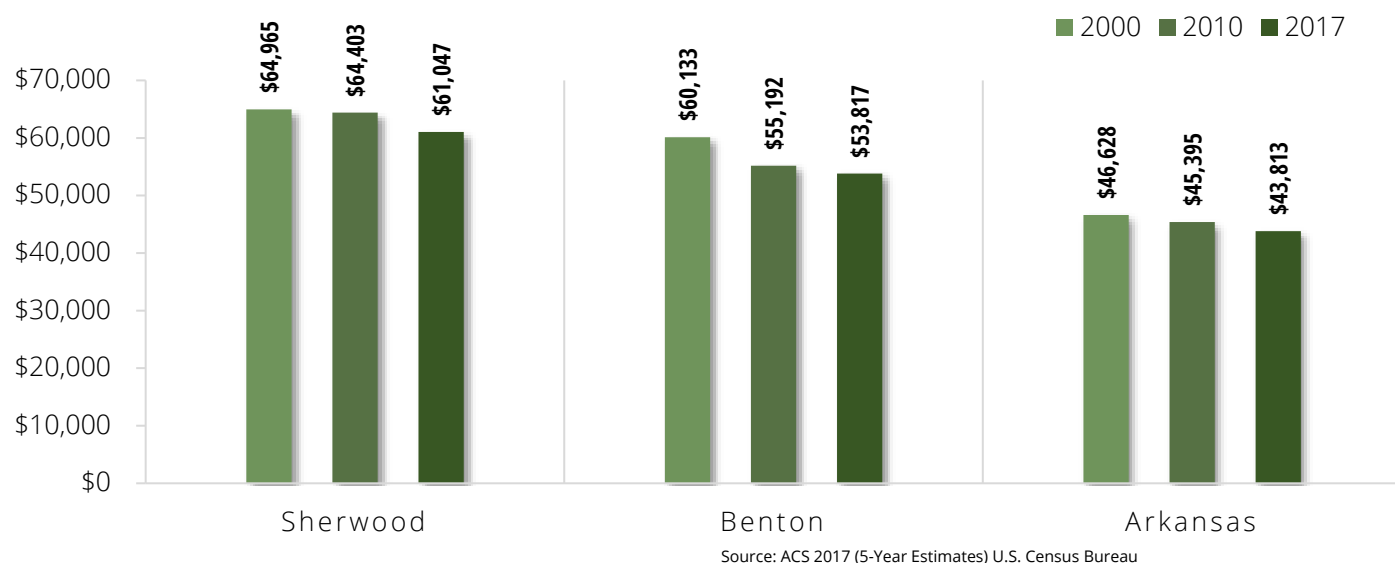


Figure 2.2.8 Per Capita Income – Sherwood and Selected Entities (2017 Dollars)

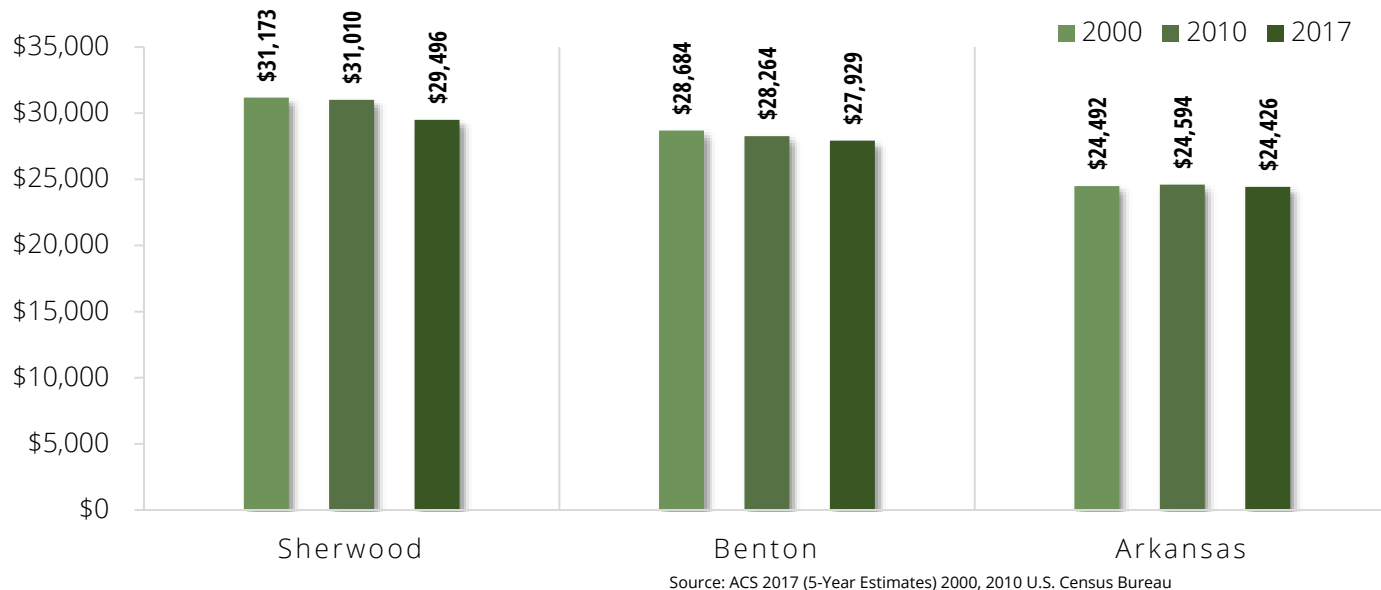


Figure 2.2.9 Household Income Distribution – Sherwood and Arkansas

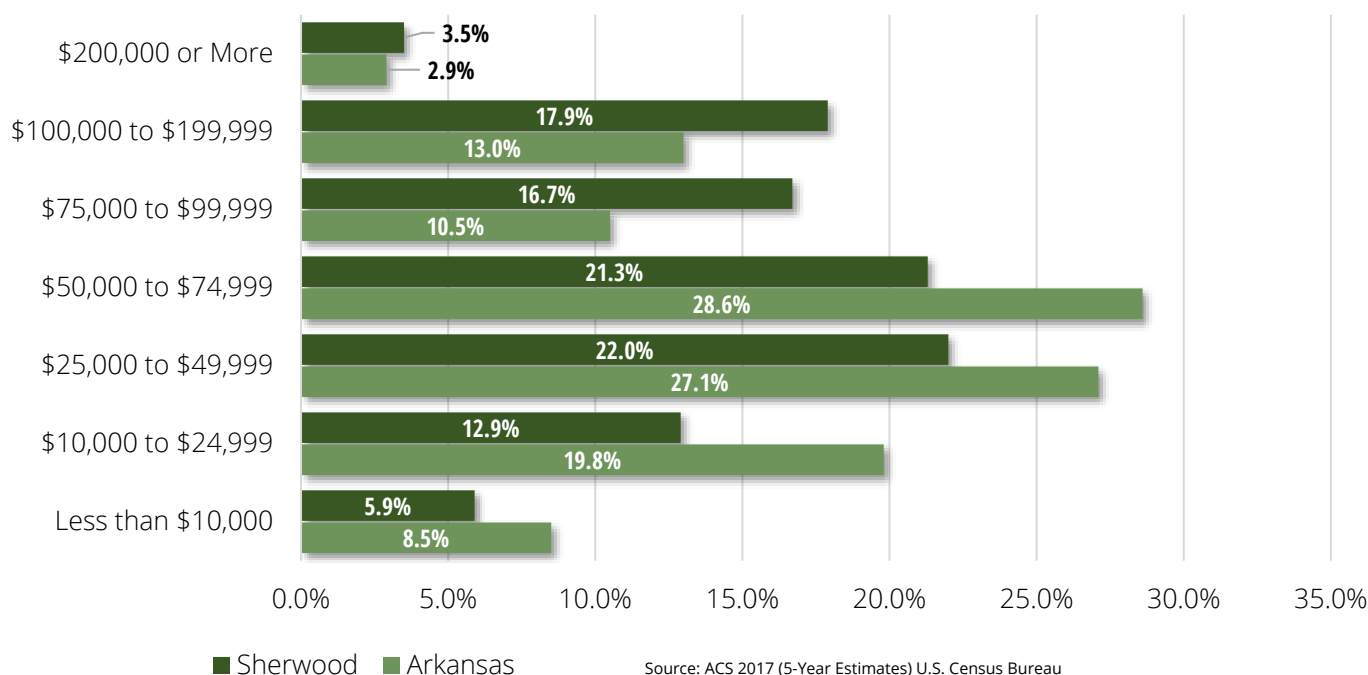


Table 2.2.10 Poverty Status – Sherwood and Selected Entities

Poverty Category	Sherwood	Benton	Arkansas
Individuals below poverty level, 1999	6.3%	8.6%	15.8%
65 years and older	4.2%	11.5%	13.8%
Under 18	10.2%	9.9%	21.8%
Individuals below poverty level, 2017	↑ 11.9%	↑ 8.8%	↑ 18.1%
65 years and older	↑ 7.6%	↓ 6.2%	↓ 10.4%
Under 18	↑ 18.4%	↑ 9.9%	↑ 25.7%
Families below poverty level, 1999	5.4%	5.8%	12.0%
No Husband Present	16.0%	21.1%	34.7%
Families below poverty level, 2017	↑ 9.0%	↑ 6.5%	↑ 13.2%
No husband present	↑ 29.7%	↑ 15.8%	↑ 35.2%

Source ACS 2017 (5-Year Estimates)

- Sherwood has the third highest median household income in Central Arkansas behind Maumelle and Bryant.
- Sherwood's median household income exceeds that of the nation as whole.
- Despite high incomes, poverty rates in Sherwood are increasing. Most notably poverty rates have increased the most sharply among female led households and the population under 18.
- While poverty rates are increasing, they remain well below state averages. However, increasing poverty rates should be of concern with more investigation done as to why these rates are increasing.
- Over 1/3 of Sherwood households make between \$75,000 and \$200,000 per year.

## Housing

Figure 2.2.11 Housing Occupancy Status – Sherwood and Selected Entities

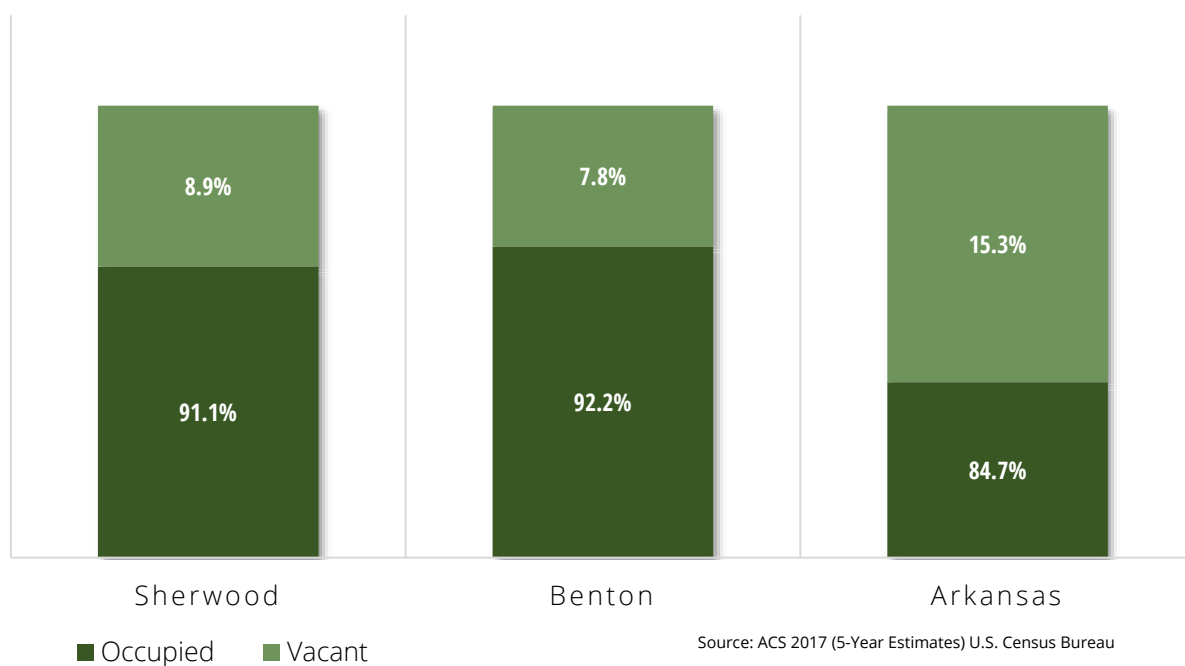


Figure 2.2.12 Housing Vacancy by Occupancy Type – Sherwood and Selected Entities

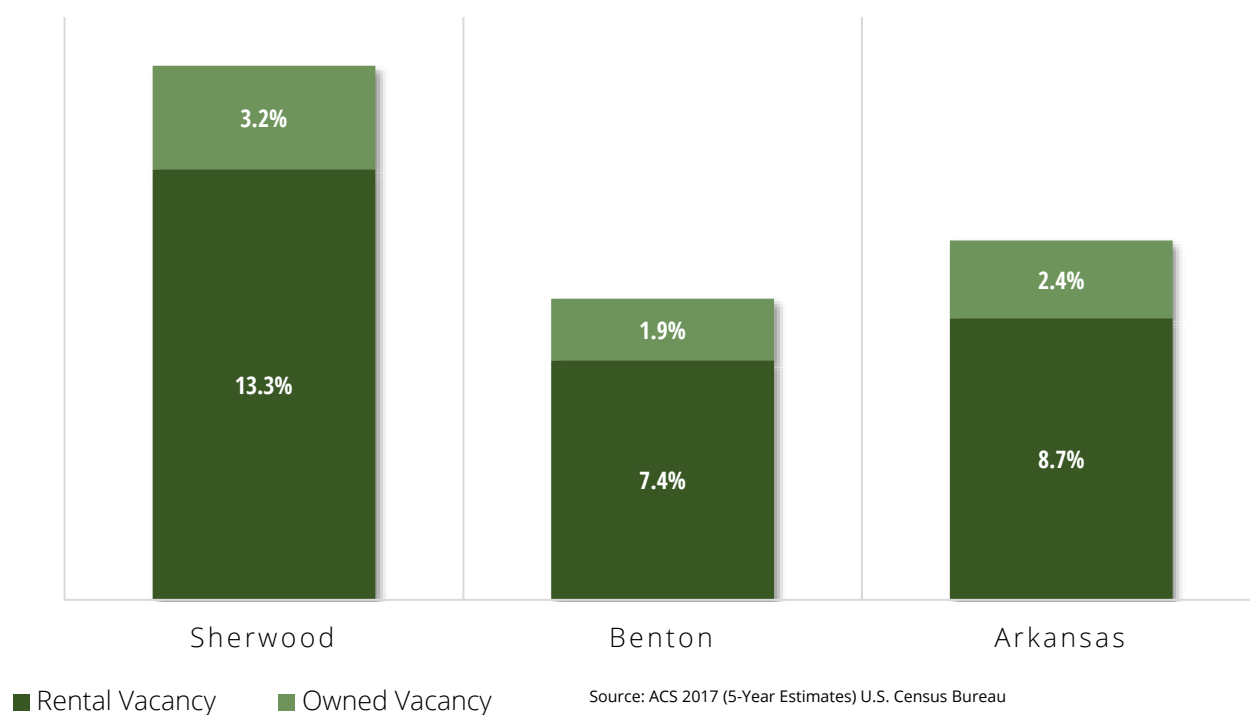


Figure 2.2.13 Percentage of Households with Housing Affordability – Sherwood and Selected Entities

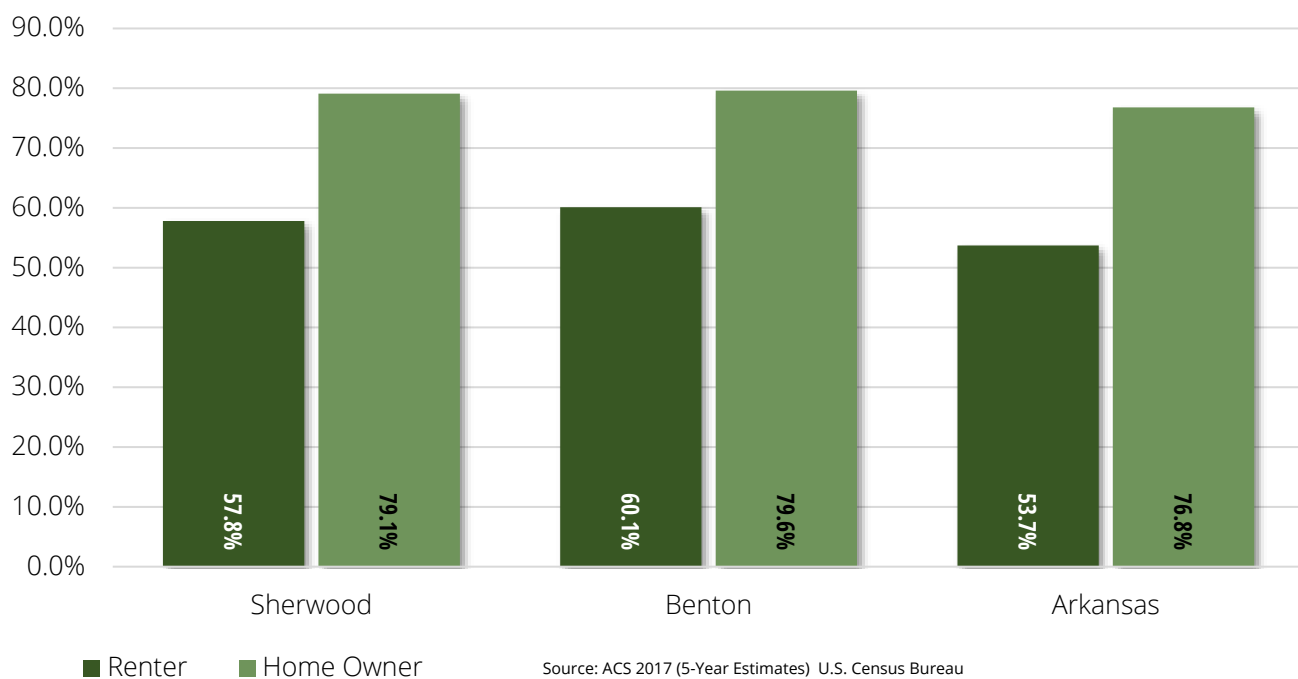


Figure 2.2.14 Age of Housing – Sherwood and Arkansas

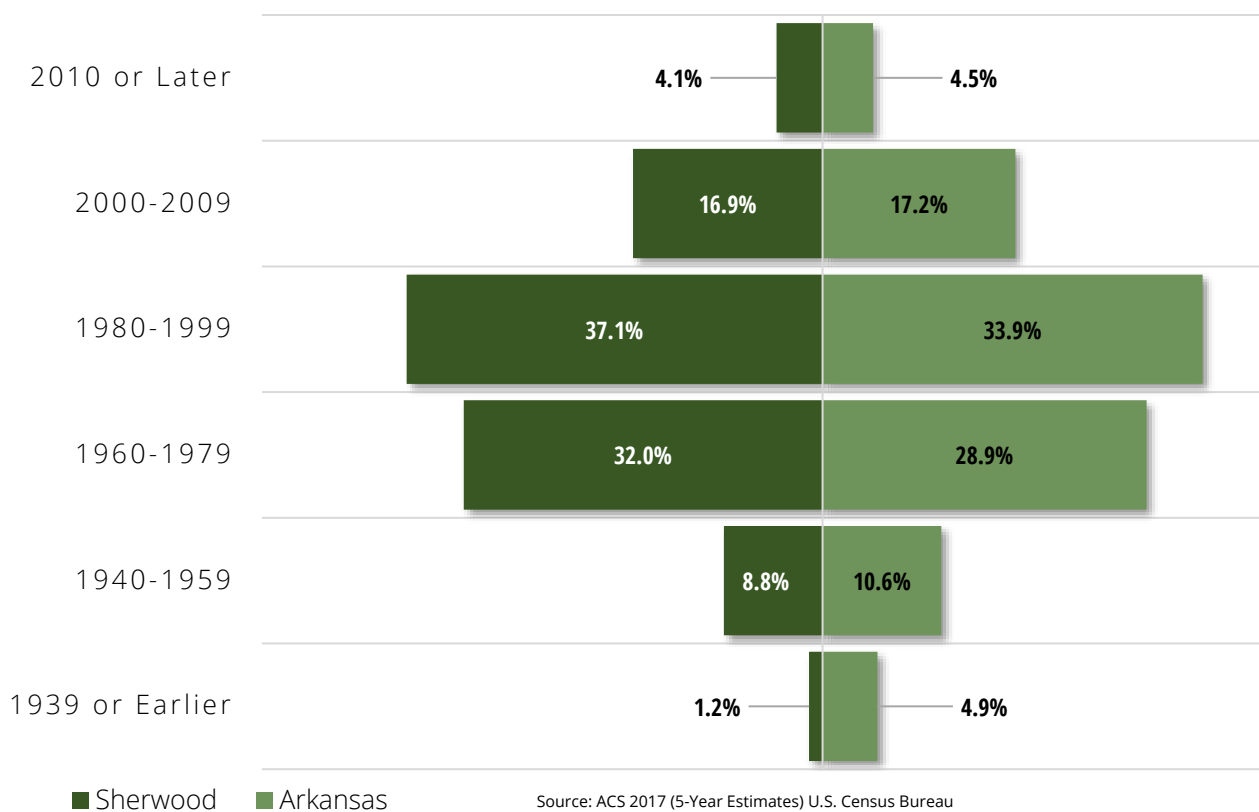
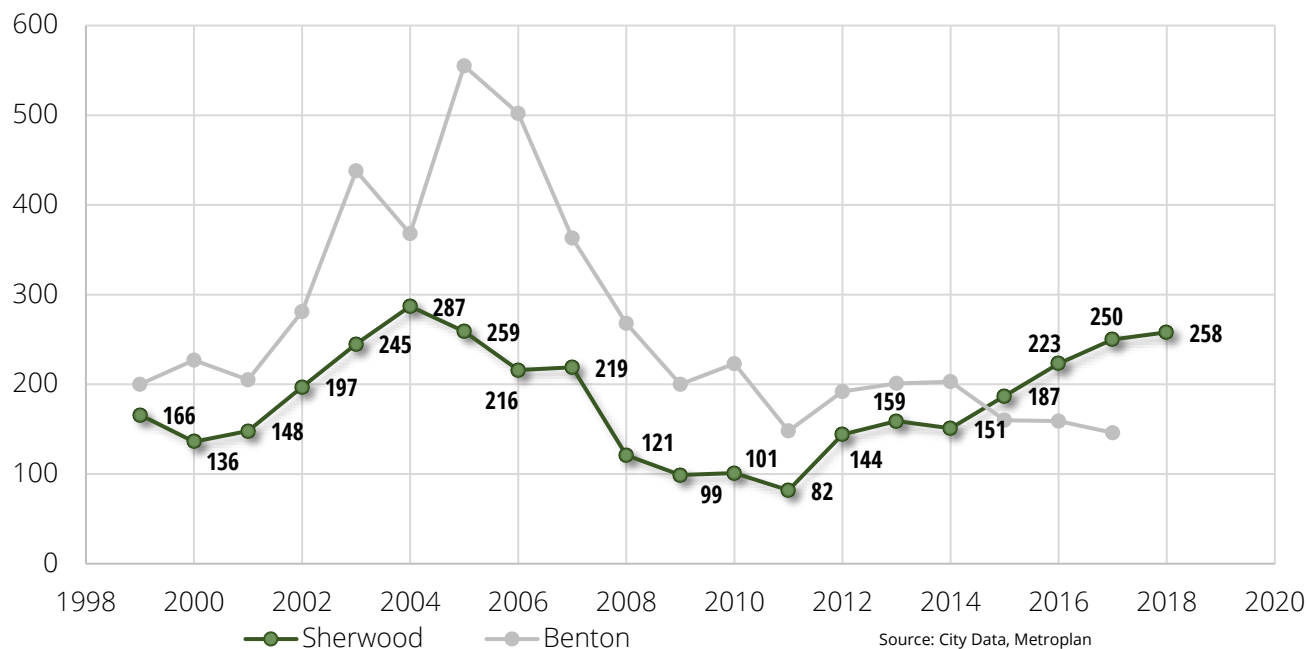


Figure 2.2.15 New Single-Family Construction – Sherwood



- Sherwood has seen a return in recent years to near pre-recession levels of single-family home construction. Between 2015 and 2017, Sherwood had more single-family home construction than any other city in the metro area except Little Rock.
- Sherwood has healthy rates of housing affordability.
- The city's rental vacancy rate is concerning, and may indicate a need to commit greater resources to code enforcement or rental inspection. It is typical when rental vacancy rates are high to see potentially poor housing conditions in portions of the rental market.

### Transportation

Figure 2.2.16 Travel Time to Work – Sherwood and Selected Entities

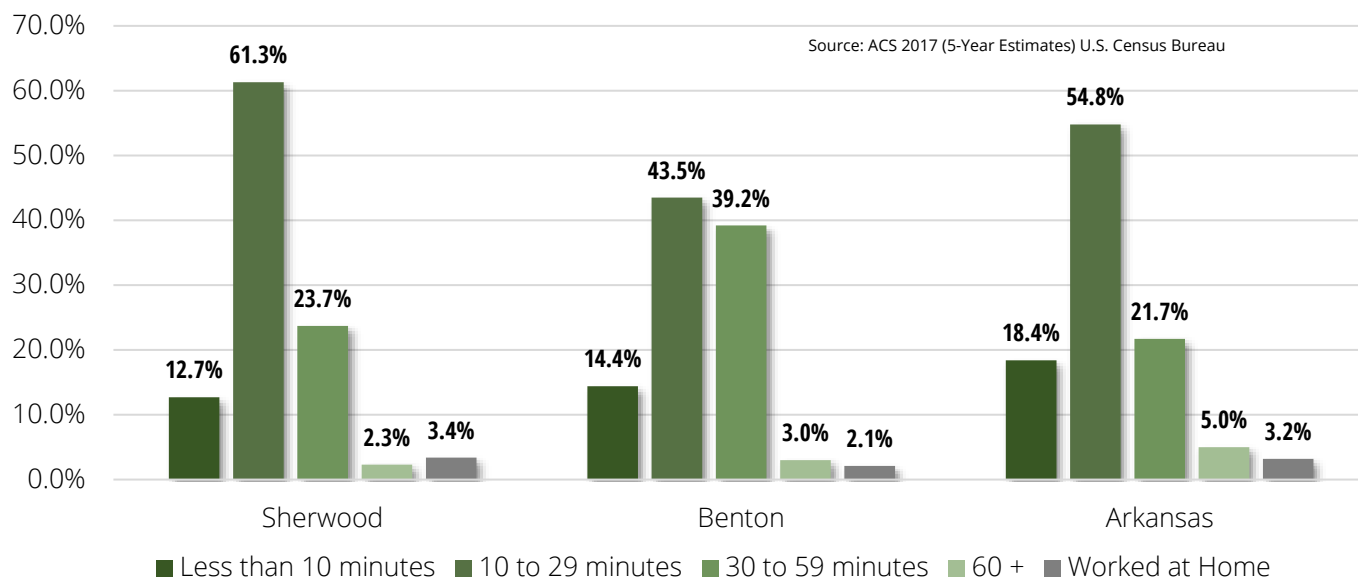
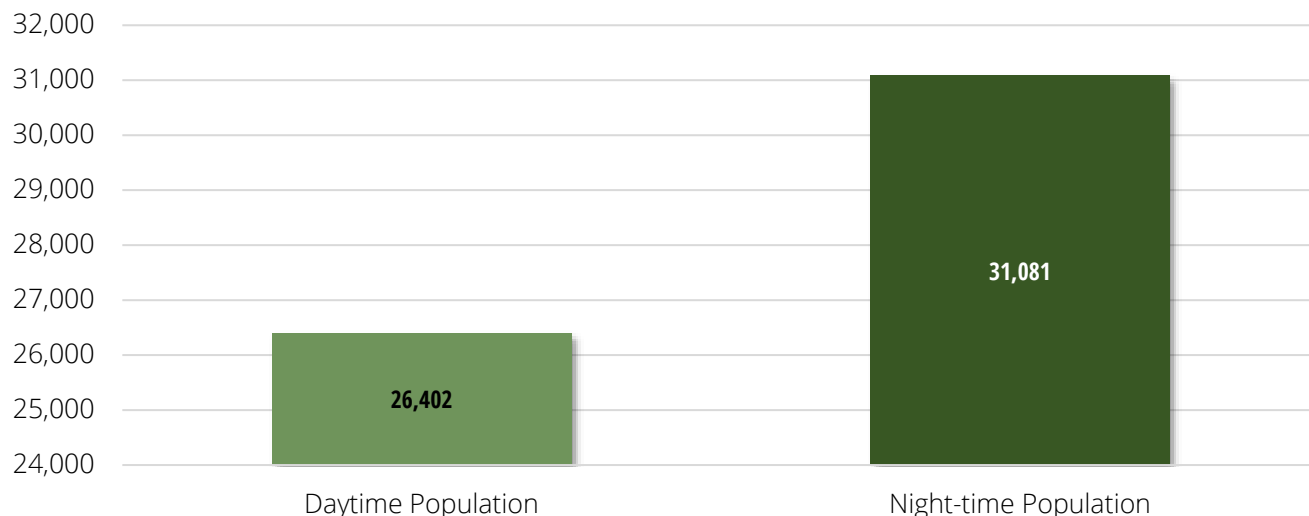
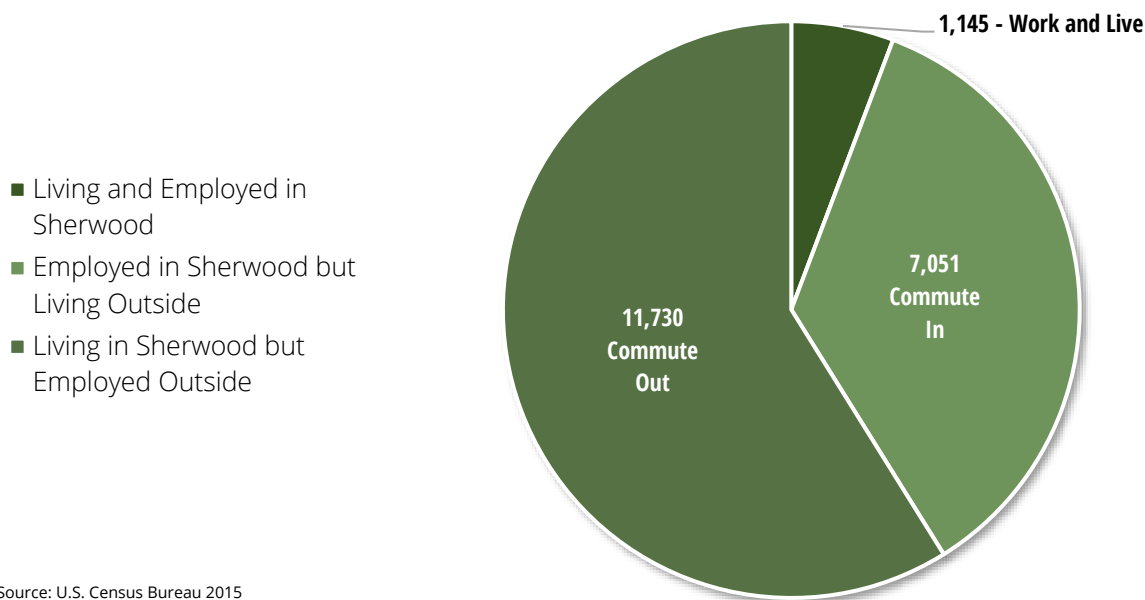


Figure 2.2.17 Daily Population Change – Sherwood



Source: ACS 2017 (5-Year Estimates) U.S. Census Bureau

Figure 2.2.18 Daily Job Inflow/Outflow – Sherwood



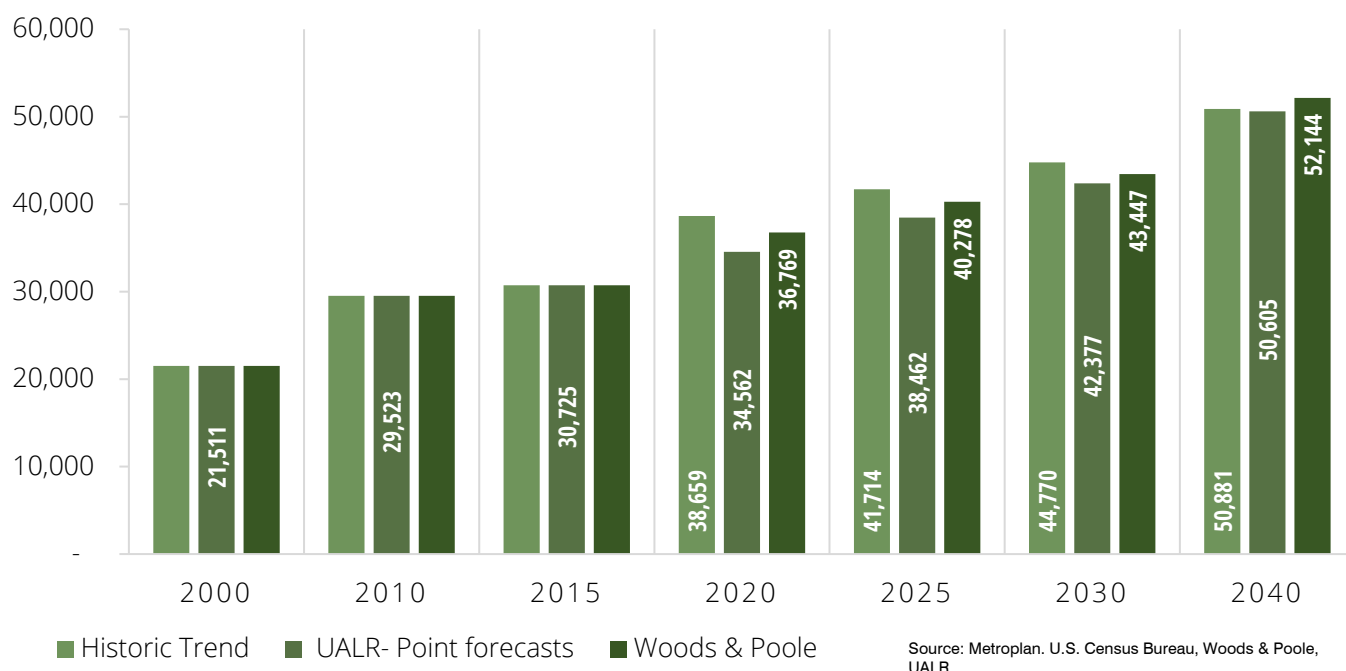
Source: U.S. Census Bureau 2015

- While Sherwood is clearly a suburb with large portions of its population commuting to Little Rock and North Little Rock, the city created over 1,500 jobs between 2005 and 2015. It is likely as the city attracts more employment, its function as a suburb may change.
- Sherwood's daytime population decreases by nearly 5,000 each day as workers commute to cities across the region for employment. Little Rock, North Little Rock, and Conway are top destinations.
- Nearly 75% of Sherwood residents have a commute of less than 30 minutes, making the city more attractive than regional peers like Benton where commute times are longer.
- Only 14% of people employed at jobs in Sherwood are residents of the city.



## Population Change Scenarios

Figure 2.2.19 Population Change Scenarios – Sherwood



Sherwood has been experiencing population growth since the 1970's, but how will Sherwood growth patterns change in the coming decades? The Woods and Poole projection shows potential population growth of nearly 13,000 people by 2030 and over 20,000 people by 2040. The UALR projection indicates similar, but more conservative, growth rates over the same period. Both projections are based on extensive data analysis at the metro area or county level applied to the city. What does the population look like if it the community continues to grow the way it has historically? The historical trend line, based on a regression analysis, shows very similar growth as the other growth models.

Most indicators show strong continued population growth in Sherwood, and current demographic trends are also supportive of this. Sherwood can influence how and where this growth will occur. Will single-family development continue to be the driving force of the city's residential growth? If so, will this growth provide adequate revenues to properly maintain city services and infrastructure? Could improvements in quality of life and sense of place developments alter the city's growth trends? All these ideas and questions need to be investigated in order to help direct the city's future.

## THREE: VISION AND GOALS

### 3.1: Vision Statement

Sherwood will be a safe, attractive COMMUNITY OF CHOICE with a strong local economy, superior infrastructure and services, and an excellent quality of life and place. The community will be one that invests in amenities for its residents and fosters a shared identity of community pride. The city will provide an equitable regulatory process that creates a strong sense of place and protects private property.

The vision is to be supported through the following guiding principles:

1. *Preserve Open Space and Environmentally Sensitive Areas.*
2. *Take Advantage of Existing Community Assets.*
3. *Foster Walkable Neighborhoods Offering a High Quality of Life.*
4. *Create a Range of Housing Opportunities and Affordable Choices.*
5. *Mix Land Uses for Pedestrian-Friendly Places.*
6. *Promote Attractive Development with a Strong Sense of Place.*
7. *Provide Recreational and Cultural Amenities*
8. *Provide a Variety of Transportation Choices and High-Quality Transportation Corridors.*
9. *Expand Economic Development Opportunities.*
10. *Ensure Policies and Processes that are Enforced and Equitable to both Citizens and Developers.*

### 3.2: Goals and Policies

The following goals and policies help direct action within the plan. These goals aid with the actualization of the vision and provide a framework for decision making by the Planning Commission and City Council.

#### Goal areas:

1. *Quality of Life/Recreation*
2. *Community Identity and Image*
3. *Growth Management and Fiscal Health*
4. *Transportation and Infrastructure*

#### ***Quality of Life/Recreation***

Quality of life and recreational amenities for a city are important for community growth in the 21<sup>st</sup> century. Generational and cultural changes in priorities for work/life balance are changing the criteria that millennials and baby boomers use in choosing where to live, making quality of life increasingly important. Today, people are moving to the place they want to live and then looking for a job. To remain competitive in attracting growth, it is essential that Sherwood provides competitive quality of life amenities for its residents.

#### Goal 1.1: Develop a connected recreational framework throughout the city.

Policy 1.1.1 – Connect all parks, schools, and large commercial areas through bike and pedestrian infrastructure to improve accessibility of amenities.

Policy 1.1.2 – Invest in parks, in areas such as North Sherwood, that are currently underserved to improve city-wide parks coverage.



Plan policies supports periodic updates and maintenance of the city's Parks and Recreation Master Plan.

Policy 1.1.3 – Maintain a Parks Master Plan to guide park development, redevelopment, and programming. This includes periodic updates.

Policy 1.1.4 – Ensure future street improvements adequately provide for pedestrians, cyclists, and drivers by including sidewalks and trails where appropriate.

Policy 1.1.5 – Develop both active and passive recreation opportunities and expand recreational programming.

Policy 1.1.6 – Pursue partnerships with community institutions to leverage the impact of city recreational facilities.

Goal 1.2: Promote the creation of neighborhoods with amenity and a sense of place.

Policy 1.2.1 – Promote compatible infill development within existing neighborhoods.

Policy 1.2.2 – Encourage new neighborhoods that are planned to combine a variety of design elements, uses, densities, housing options, and amenities.

Policy 1.2.3 – Provide land use regulations and review processes that promote mixed-use developments.

Policy 1.2.4 – Promote residential cluster development that provides common greenspace (small lots with a balance of area reserved for open space).

Goal 1.3: Promote recreational and cultural programming that enhances quality of life.

Policy 1.3.1 – Provide quality public space for cultural events that have live music and specialty foods to help bring the community together.

Policy 1.3.2 – Encourage ways to include art within the built environment and foster outlets for local performing arts.

Policy 1.3.3 – Pursue partnerships with community institutions to provide cultural programming that can have a community-wide benefit.

Policy 1.3.4 – Encourage community institutions to provide festivals and programming that enhances quality of life.

Policy 1.3.5 – Support city recreation programs in addition to community recreational programs.

### ***Community Identity and Image***

A hallmark of suburban development is the sameness and lack of identity it produces within communities. As a product of the era of suburban development, Sherwood suffers from these issues. The community has no focal point that embodies its identity or definable edges that differentiate it from surrounding communities. Addressing these issues will be critical to ensuring Sherwood maintains stability as it continues to grow.

#### *Goal 2.1: Promote regulations and development that fosters a sense of local identity.*

Policy 2.1.1 – Encourage the preservation of the Old Sherwood neighborhood in the city's southwest section.

Policy 2.1.2 – Stimulate redevelopment of Kiehl Avenue and U.S. 67/167 to provide a more positive image of Sherwood.

***Plan policies support development and redevelopment that fosters a strong sense of local identity unique to Sherwood.***

Policy 2.1.3 – Ensure developments provide for sensitive and compatible transitions between residential and nonresidential areas.

Policy 2.1.4 – Establish standards for high-quality design of multi-family housing.

Policy 2.1.5 – Establish commercial design standards that provide for a unique “Sherwood Character”.

Policy 2.1.6 – Pursue events and outreach opportunities that communicate Sherwood's history and identity.

#### *Goal 2.2: Promote development that provides Sherwood a community focal point and definable city edges.*

Policy 2.2.1 – Promote and encourage the construction of a Town Center style development near the intersection of Brockington Road and Highway 107.

Policy 2.2.2 – Explore ways to enhance the existing city civic complex to create a central community-gathering place and focal point.

Policy 2.2.3 – Adopt regulations to enhance the city's key image gateways such as U.S. 67/167, Brockington Road, Kiehl Avenue, and Highway 107.

Policy 2.2.4 – Pursue regulations that encourage high-quality developments that will help better define the borders between Sherwood and North Little Rock.

#### *Goal 2.3: Use code enforcement as a means to enhance community image.*

Policy 2.3.1 – Use city resources to enforce the city's zoning and subdivision regulations.

Policy 2.3.2 – Focus code enforcement efforts on neighborhoods where blighting influences could serve to destabilize property values.

Policy 2.3.3 – Use a collaborative approach with property owners in addressing code enforcement violations by helping property owners in need identify resources to assist them.

Policy 2.3.4 – To use the provisions of Arkansas law such as the “City Cleanup Tools” Act and other laws to address nuisances and unsightly, unsafe, and unsanitary conditions on private property.

Policy 2.3.5 – Use the condemnation process to remove abandoned and dilapidated structures that serve to harm the city’s neighborhoods.

*Goal 2.4: Support the creation of an independent Sherwood School District.*

Policy 2.4.1 – Provide support to groups working to create a Sherwood School District.

Policy 2.4.2 – Support those schools that serve Sherwood residents.



***Growth Management and Fiscal Health***

How will Sherwood develop and change as it grows? Sherwood has changed dramatically in the last 20 years and must continue to evolve. Growth can bring many difficulties including infrastructure and fiscal challenges. A well-crafted and intentional growth strategy will build a strong foundation for Sherwood’s future as it regulates development, invests tax dollars, works to attract new businesses and residents, and builds in a way that create long-term fiscal health for the community.

*Goal 3.1: Guide and carefully direct growth in a smart and responsible manner.*

Policy 3.1.1 – Encourage development in areas already served by city services, where service provision is most cost effective.

Policy 3.1.2 – Ensure new developments locate where they can be accommodated by existing public infrastructure.

Policy 3.1.3 – Encourage development that is compatible with the natural and built environment of the surrounding area.

Policy 3.1.4 – Encourage development that creates long-term community value and adapts well for future re-use.

Policy 3.1.5 – Promote development that builds the city’s tax base and generates enough tax revenue to pay for the life-cycle costs of its supporting infrastructure.

Policy 3.1.6 – Create thriving, vibrant neighborhoods, districts, and corridors that are distinct places.

Policy 3.1.7 – Promote mixed-use development that combines commercial, residential, and office functions in the same building.

Goal 3.2: Adopt, maintain, and use Sherwood Vision 2040 as the city's comprehensive plan.

Policy 3.2.1 – The planning commission will conduct annual reviews of its comprehensive plan and land use regulations to ensure they remain applicable and up-to-date.

Policy 3.2.2 – The city's land use regulations will be consistent with and designed to carry out the provisions of the comprehensive plan.

Policy 3.2.3 – Development proposals will be evaluated in terms of their compatibility with the comprehensive plan.

Policy 3.2.4 – All amendments to the city's land use regulations, including rezoning requests, must conform to the comprehensive plan.

Goal 3.3: Enhance the city's land use regulations and development review processes.

Policy 3.3.1 – Provide and carry out land use and building regulations that protect the health, safety, welfare, and aesthetics of the community.

***Significant updates to the city's land use regulations are needed to help carry out the goals and policies of this plan.***

Policy 3.3.2 – Use innovative regulatory tools designed to address community problems identified in the plan such as building design standards, landscaping requirements, character zoning, etc.

Policy 3.3.3 – Promote use of land use tools that allow flexibility in site design and layout for innovative developments.

Policy 3.3.4 – Support policies that allow the construction of a variety of housing types and price ranges to meet the needs of residents of all ages and incomes.

Policy 3.3.5 – Provide timely and efficient review of development proposals.

Policy 3.3.6 – Provide fair, consistent, and transparent review and evaluation of all development proposals.

Policy 3.3.7 – Ensure development review is conducted in way that evaluates external impacts of development on adjacent properties.

Policy 3.3.8 – Ensure that the planning commission is representative of the larger community.

Goal 3.4: Coordinate growth and utilities in the most efficient and effective manner.

Policy 3.4.1 – Control the extension or provision of utilities in order to carry out the provisions of this plan.

Policy 3.4.2 – Ensure no approved development will result in a reduction in the adopted level of service for public infrastructure.

Policy 3.4.3 – Require all developments within the Planning Area Boundary to be served by central water and wastewater services, when technically feasible.

Policy 3.4.4 – Require all developments to install public utilities and become annexed to the city as a condition of tying onto city utilities.

Policy 3.4.5 – Encourage development to occur where it can be supported by the transportation and utility infrastructure.

Policy 3.4.6 – Prioritize the maintenance of existing utility and transportation infrastructure over the expansion of new facilities.

Policy 3.4.7 – Promote development patterns such as mixed-use development that yield higher tax revenue per acre.

Policy 3.4.8 – Ensure local tax policies provide adequate revenue to meet the city's ongoing liabilities.

*Goal 3.5: Adequately address issues of drainage and flood hazards.*

Policy 3.5.1 – Ensure developments adequately address drainage to ensure new drainage problems are not created.

Policy 3.5.2 – Promote the use of green infrastructure as a way to work with the environment to prevent localized flooding risks and drainage problems.

Policy 3.5.3 – Provide regulations that ensure drainage issues caused by development are properly mitigated.

Policy 3.5.4 – Pursue innovative programs and policies like a stormwater utility to ensure adequate mechanisms are in place to finance public investment in drainage infrastructure.

***Transportation and Infrastructure***

Transportation is an important issue for Sherwood's future growth. In order to remain a competitive marketplace for new residents and job growth, the city's transportation network will continue to need investment. Better transportation connections between the Gravel Ridge area of North Sherwood and U.S. 67/167 as well as across Kellogg Creek will become vitally important, with new corridors necessary to ensure existing corridors do not become gridlocked. Failure to address these issues adequately could significantly impede future growth.

*Goal 4.1: Provide a transportation system that is safe and efficient.*

Policy 4.1.1 – Focus transportation infrastructure investments on corridors that will relieve traffic and improve connectivity.

Policy 4.1.2 – Improve connectivity between North Sherwood and US. 67/167.

Policy 4.1.3 – New developments must provide for the interconnection of existing and proposed streets to permit the orderly expansion of the city's transportation system.

Policy 4.1.4 – Proposed subdivisions, developments, site plans, or concept plans must comply with this plan. The Planning Commission may consider, on a case by case basis, innovative designs that promote desirable developments without sacrificing the overall goals of this plan, other city plans, or the Arkansas Fire Prevention Code.



Policy 4.1.5 – New developments adjacent to or encompassing existing streets shall be responsible for construction of half-street improvements to those streets. Those improvements will be consistent with the functional classification reflected in this plan, the city's construction standards, and all other applicable standards.

Policy 4.1.6 – Where new developments are adjacent to or encompass arterial streets controlled by the Arkansas Department of Transportation (ArDOT), half-street improvements shall meet the functional classification shown on the Master Street Plan or as determined by agreement between the developer, the ArDOT, and the city.

Policy 4.1.7 – Strip commercial developments are discouraged and may be subject to access limitations. The city encourages commercial developments to provide their own internal streets and drives for direct access to individual outparcels.

Goal 4.2: Provide a transportation system that is equitable and benefits all residents.

Policy 4.2.1 – Bike and pedestrian facilities will be constructed as part of all new development and transportation facilities according to the provisions of this Plan.

Policy 4.2.2 – Bike and pedestrian users will be given consideration in the planning and design of all transportation facilities in the planning area.

Policy 4.2.3 – The city will carefully monitor mobility and access options for citizens with disabilities when reviewing development proposals.

Policy 4.2.4 – The city will develop a bike and pedestrian transportation system that will consider the mobility and safety needs of a variety of uses including children, seniors, active adults, and the physically challenged.

Policy 4.2.5 – Utilize context sensitive roadway design approaches to ensure roadways are appropriate for the function of the supporting land use.

Goal 4.3: Utilize access management to preserve the function of existing and future corridors.

Policy 4.3.1 – Access management principles shall be incorporated in the city's land use regulations.

Policy 4.3.2 – Access points for individual properties fronting collector, minor arterial, and major arterial streets shall be kept to a minimum to facilitate traffic movement, reduce crashes and fatalities, and to increase market areas for local businesses. Keeping access points to a minimum may be achieved through driveway consolidation, joint access agreements, or specific corridor access management plans.

*Access management is an important tool to help ensure the city's traffic flows in a safe and efficient manner.*

Policy 4.3.3 – The staff and planning commission shall include considerations of access management principles in the review of all development plans or requests.

### 3.3: Community Outreach

#### **Community Engagement Process**

Community engagement factors heavily into the findings and recommendations of this plan. This is because Sherwood residents and stakeholders ultimately know their community better than anyone. Accordingly, the plan process provided numerous opportunities for community engagement. These included:

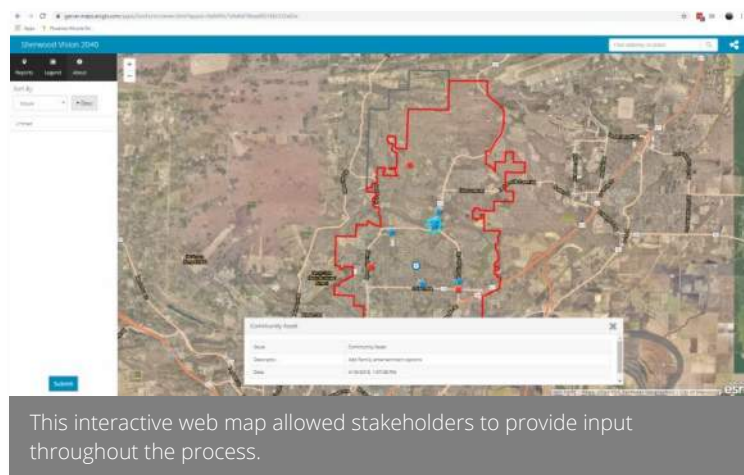
*Community engagement factors heavily into the findings and recommendation of this plan. Ultimately, Sherwood residents and stakeholders know their community better than anyone.*

- Public Steering Committee
- Community Workshop
- Design Preference Survey
- Steering Committee Workshop
- Interactive Website
- Public Plan Review and Presentations

#### **Community Engagement Findings**

The following contain the community input provided from several sources including the Vision Sherwood Community Workshop. The issues and recommendations range over a variety of topics; however, a few key themes emerged.

**Transportation** – Transportation was identified as a key issue within the kick-off meetings. The circulatory system for traffic in Sherwood needs improvements and interconnectivity to move trips from east to west as well as north to south. More connections are needed to US 67/167 as bottle necks are created at Brockington/Kiehl and Wildwood does not serve as a true arterial connection. Transportation will remain one of the key focuses of the plan in order to enhance pedestrian and vehicular mobility.



**Quality of Life/Recreation** – Quality of Life/Recreation was brought up in many of the comments from the charrette. Building on the success of recreation and quality of life improvements that have occurred in recent years citizens

want to see an expansion of the trail system, investment in Kellogg Creek as a natural area, a town center, and focus on retail/entertainment offerings. Because of continued community interest, it is certain addressing quality of life will be an important component of this plan.



**Code Enforcement/Community Image** – Several groups identified two neighborhoods as needing code enforcement and reinvestment. Additionally, groups commented on a need to solidify the community's identity. The city does not have a strong enough connection to old Sherwood, and the historic area does not rest in the central part of the community. Focus will be given on ways to enhance community identity and spur reinvestment in blighted neighborhoods.

## FOUR: THE PLAN

### 4.1: Place Type Plan Standards and Study Areas

#### ***Introduction***

Many land use plans focus on the separation of different types of land uses. This approach often fails to consider whether proposed land uses actually create any adverse influence on one another. In other words, the strict separation of land use types becomes an end in itself and not an element of the urban design process. In departure from this approach, this plan emphasizes the nature of land uses. Thus, the size, use intensity, traffic generation, and the overall impacts of a development become more important than the actual activity conducted on the property. The final product is the future place type map that will inform zoning in the city based upon the place type transect shown below.

#### ***Natural***

These place types are designed to protect areas which are intended for recreational development or are environmentally sensitive and prone to natural hazards such as flooding. It is understood preservation of these areas serves a necessary purpose to promote and protect community health, safety, and welfare.

##### Conservation Area (N-CON)

*Character:* Areas intended for natural preservation and resource conservation. These areas include environmentally sensitive areas such as floodplains/floodways, wetlands, or areas with slopes exceeding 20%. Development is not intended for these areas. However, uses that have little or no impact on the surrounding areas, or can be adequately mitigated may be allowed. These areas may feature passive recreation facilities such as trails, or the related zoning may be used as a buffer device between adjacent properties/uses.

*Land Use:* Vacant/Recreational

*Density:* Not applicable

*Utility Requirements:* None

*Related Zoning:* OS



### Recreational Area (N-REC)

*Character:* Areas intended for recreational use. These uses include golf courses, soccer fields, baseball/softball complexes, and other similar recreational facilities. The natural function of land in this area should be preserved. When alterations to the physical form of the land are required for areas subject to flooding and natural hazards, changes should be mitigated.

*Land Use:* Recreational/Vacant

*Density:* Sparsely spread buildings that meet public functions or park needs.

*Utility Requirements:* Possible utility requirements

*Related Zoning:* All Districts



### **Rural**

Certain areas belong inside the corporate limits in order to promote healthy development patterns of the future while guarding against those that would create a blighting influence on the planning area. In particular, these include areas that to are not yet served by municipal wastewater systems.

### Rural Reserve (R-RSV)

*Character:* Areas in the periphery of the city devoted to primarily residential use or forestry and agriculture with low impact commercial and institutional uses. This place type has no uniform development pattern. There are a range of housing types and lot sizes. Platted residential areas are not programmed for sewer service. Unplatted areas and farms/forest are encouraged to remain in large parcels under common ownership to allow for future development. This development pattern encourages commercial and institutional uses to congregate at intersection nodes.

*Land Use:* Agricultural/Rural Residential

*Density:* 2 acres + for Agricultural, Single-Family Residential (excluding manufactured homes)

*Utility Requirements:* Water

*Related Zoning:* R-A (New)





## ***Suburban***

Because of their dependence on automobiles and inefficient use of land and infrastructure, many aspects of suburban development create long-term financial problems for communities. However, market demand continues to create the necessity to plan for suburban development. Negative impacts of these areas should be mitigated through improved design to increase landscape aesthetics, livability, and financial sustainability.

### *Suburban Single-Family (S-SFR)*

*Character:* Areas set aside for neighborhoods of single-family homes. These include low density subdivisions that have already been developed or those areas intended for subdivision development. These areas are mostly made up of single-family detached homes with driveways and garages/accessory structures along with customary institutional uses such as schools and places of worship. These neighborhoods have auto-oriented functions.

*General uses:* Single-family homes, churches, parks

*Density:* 3-6 units/acre

*Utility requirements:* Sewer and water services

*Appropriate zoning:* R-1



### *Small Scale Multi-Family (S-MFR)*

*Character:* Areas intended primarily for mixed density housing developments incorporating single-family and small-scale multi-family residential in cohesive neighborhoods with customary institutional uses such as schools and places of worship. The areas also feature small-scale multi-family residential such as single duplex or four-plex units on an individual lot. These neighborhoods have auto oriented functions, and are intended for location generally along local streets and/or collector streets.

*Land Use:* Small-Scale MF, Mixed-Density Res.

*Density:* 18 units per acre or less

*Utility Requirements:* Sewer/Water

*Related Zoning:* R-2



### Large Scale Multi-Family (S-APT)

*Character:* Areas intended primarily for large-scale multi-family residential such as garden-style apartment complexes. These areas also include a mixture of housing types including single-family, two-family, and small-scale multi-family residential. Where single-family and two-family housing is included, it is intended to be located within a subdivision. These areas are typically characterized by one to four-story buildings with medium-sized parking lots and are intended to locate along collector or arterial streets.

*Land Use:* Large-Scale/Mixed Multi-Family

*Density:* 24 units per acre or less

*Utility Requirements:* Sewer/Water

*Related Zoning:* R-3



### Manufactured Home Residential (S-MHR)

*Character:* Area intended for single-family homes, manufactured homes, and manufactured home parks. These areas provide a diversity of housing choice and incorporate a mix of densities and residential uses following a more generally scattered pattern of land use.

*Land Use:* Manu. Homes/Manu. Home Parks

*Density:* 3-8 units per acre

*Utility Requirements:* Sewer/Water

*Related Zoning:* R-4, MHP



### Office/Buffer Area (S-OFF)

*Character:* Areas include a mixture of office, light commercial uses and various housing types including duplexes, small-scale multi-family, single-family attached homes, and single-family detached homes. The districts typically represent areas in transition from single-family residential to more intensive uses or areas which office and limited commercial is most appropriate. Within these transitional areas, a great deal of care is warranted in order to balance the pressure to allow more productive uses with the need to protect existing property owners.

*Land Use:* Mix-Density Res., Ltd. Comm./Office

*Density:* 12 units per acre or less, 0.5 FAR

*Utility Requirements:* Sewer/Water

*Related Zoning:* O-1, O-2



### Commercial Corridor (S-COR)

*Character:* Areas include commercial, retail, and office developments abutting arterial corridors. These businesses are often set back with parking in front, though side/rear parking is encouraged. Typically auto-oriented, these areas consist of mainly low-rise buildings. All development should enhance the visual appearance of the city. Special regulations for design, signage, and landscaping may apply. Alternatives to simple “strip” commercial development are encouraged. The zoning designation for these areas shall depend on the site’s compatibility for intense uses, with less intense zoning areas meant for less intense development due to surrounding property.

*Land Use:* Retail, Office, General Commercial

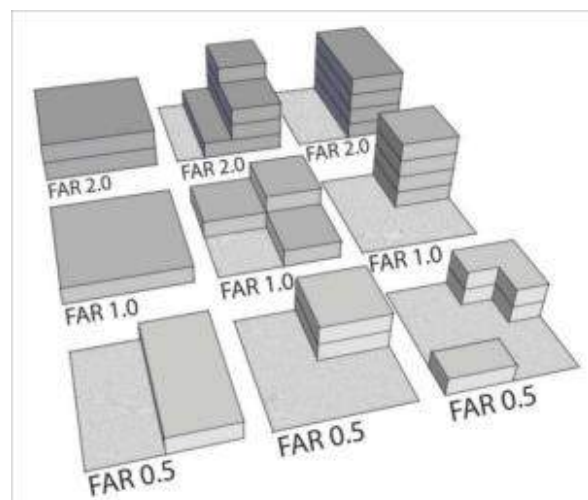
*Density:* 0.5 FAR

*Utility Requirements:* Sewer/Water

*Related Zoning:* C-2, C-3



*Floor area ratio (FAR) is the ratio of a building's total floor area (gross floor area) to the size of the piece of land upon which it is built.*





### Commercial Center (S-CEN)

*Character:* Areas include large-scale office, retail, and commercial operations on large sites abutting arterial corridors. These areas often compose commercial centers with multiple building sites including big-box stores with expansive parking lots. Outdoor storage retail operations such as big box hardware stores are also common, but all outdoor storage should be screened. All development should enhance the visual appearance of the city and be well landscaped. Special regulations for design, signage, and landscaping may apply.

*Land Use:* Retail, Office, General Commercial

*Density:* 0.5 FAR, Typically 3 acres +

*Utility Requirements:* Sewer/Water

*Related Zoning:* C-3



### Highway Corridor (S-HWY)

*Character:* Areas include medium and large-scale retail, and commercial operations along the US 67/167 corridor. These areas often feature uses such as car, boat, or RV dealerships with outside storage and/or outdoor display of products with expansive parking lots. All development should enhance the visual appearance of the city and be well landscaped. Special regulations for design, signage, and landscaping may apply.

*Land Use:* Retail, Outdoor Display Commercial

*Density:* 0.5 FAR, Typically 3 acres +

*Utility Requirements:* Sewer/Water

*Related Zoning:* C-4



## Urban

When the city has a well-developed infrastructure along with adequate municipal services, it benefits the community for the area to be developed to urban density. This will include redevelopment of properties and emphasize walkability and livability. It is vital that transportation systems be carefully coordinated with development. If needed, traffic studies will accompany development plans. Planned Unit Developments will be particularly appropriate in these sectors.

### Urban Neighborhood Center (U-NHD)

*Character:* Commercial nodes meant to provide mixed-use and small-scale commercial functions. These areas are connected to abutting neighborhoods through pedestrian amenities. The buildings front the street with on street parking and have parking in rear of commercial buildings. Residential uses also exist in this area and may be located above commercial structures. Streetscapes are well designed and there are public spaces.

*Land Use:* Mixed-Use

*Density:* 1.8 FAR Min.

*Utility Requirements:* Sewer/Water

*Related Zoning:* U-C (New)



### Traditional Neighborhood (U-TND)

*Character:* These neighborhoods comprise “Historic Sherwood” in the vicinity of streets like Sherwood and Delmont Avenues, and were developed before the automobile influenced land use. These areas are characterized by a mixture of single-family homes and well-designed historic small-scale multi-family development. Houses may or may not have parking via a garage at the rear, side, or in the front of the house. However, garages are intended to be less prominent and respect historic building forms. Lot sizes are not uniform, but larger lots are less common. Infill development is encouraged through well-designed small-scale multi-family, accessory dwelling units, and office uses within existing homes.

*Land Use:* Single-Family Res., Small-scale Multi-Family, Office

*Density:* 10 units per acre or less, 0.5 FAR

*Utility Requirements:* Sewer/Water

*Related Zoning:* R-T (New Zone)



### Walkable Neighborhood (U-WND)

*Character:* Medium density residential neighborhood characterized by development abutting the street. Garages exist but are at back of the houses or even with the front porch. The road has on street parking and there are sidewalks and other pedestrian amenities. Town houses or other multi family structures are mixed in with single-family housing. These areas can be denser than traditional urban neighborhoods depending on how much multi-family housing is constructed.

*Land Use:* Single-Family Residential, Small-Scale Multi-Family

*Density:* 12 units per acre or less

*Utility Requirements:* Sewer and water services

*Related Zoning:* R-T (New Zone)



### ***Special***

Certain areas of the community are unique and do not mesh with traditional urban or suburban forms. These include areas such as university campuses or industrial development. Such areas demand protection from adverse uses and may have a unique thematic character or land use pattern.

### City Plaza District (S-CPD)

*Character:* Area comprising a mix of government offices uses with other office/residential uses. Intended to develop a central community focal point with large public gathering places. Emphasis is strongly placed on the visual quality of supporting development with strong walkability.

*Land Use:* Public, Offices

*Density:* 1.8 FAR Min.

*Utility Requirements:* Sewer and water service

*Related Zoning:* U-C (New Zone)





### Institutional Campus (S-INS)

*Character:* These areas are composed of large-scale campuses. These campuses could be schools, medical complexes, or other similar uses. These uses are typically disconnected from other uses. Uses have an internal focus with clustered buildings that are repetitive in use and or design.

*Land Use:* Office, Public, Institutional

*Density:* N/A

*Utility Requirements:* Sewer and water service

*Related Zoning:* R/C Districts



### Industrial Development (S-IND)

*Character:* Industrial complexes made up of large-scale industrial operations. These areas preclude other types of uses from occurring. The developments are internally focused and usually require large scale buffering and landscaping between adjacent uses. Typically located away from the city center, but close to major transportation amenities.

*Land Use:* Industrial

*Density:* 1 acre +

*Utility Requirements:* Sewer and water service

*Related Zoning:* I-1



### Utility Area (S-UTL)

*Character:* Areas intended for utility uses such as water/sewer treatment plants or large easement corridors. Uses may or may not require buffering, due to differing impacts on adjacent property. Uses are required to ensure city life functions, but care should be taken regarding placement to minimize impacts on adjacent property.

*Land Use:* Water/sewer treatment plants, electrical substations, easements

*Density:* N/A

*Utility Requirements:* N/A

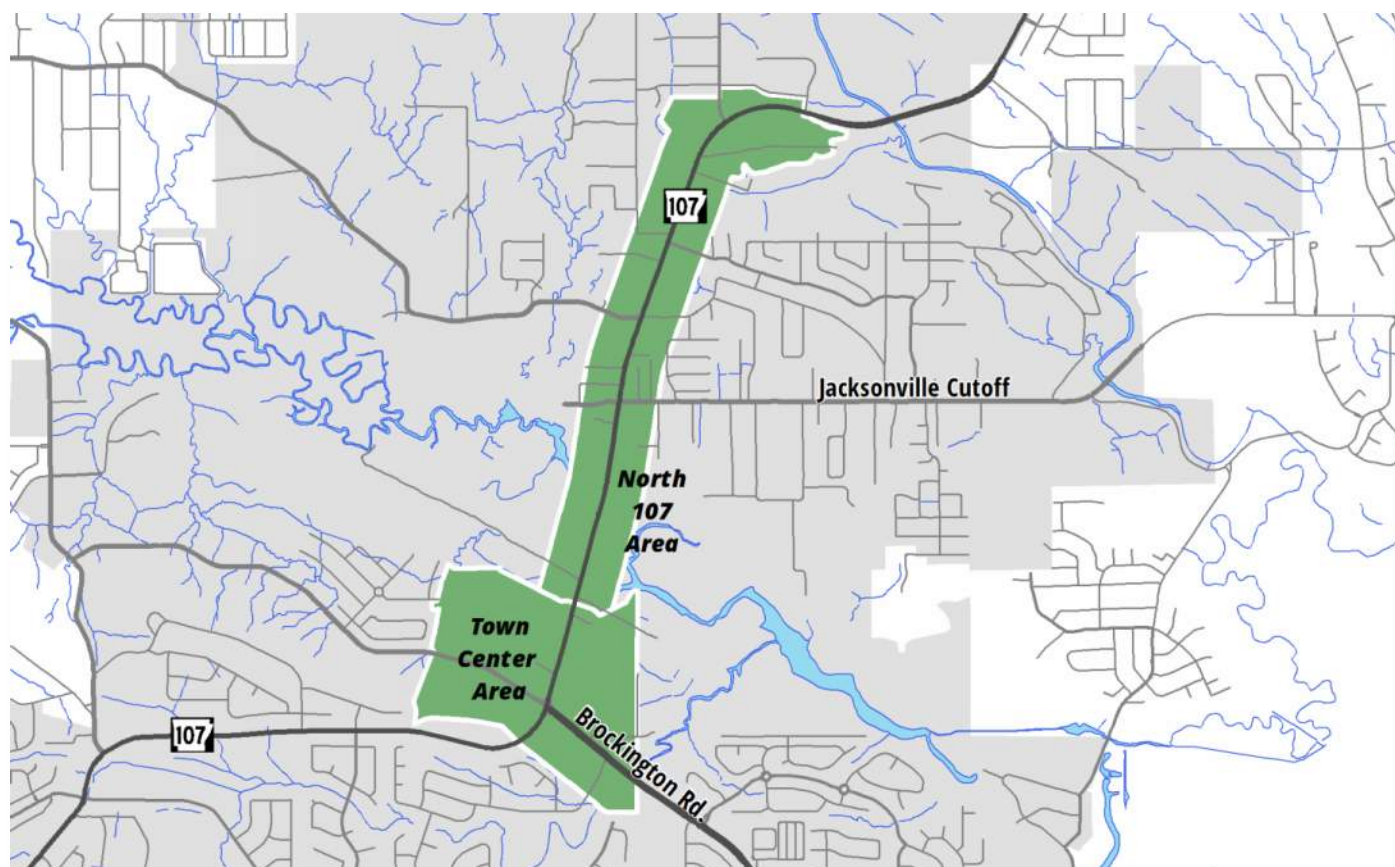
*Related Zoning:* Typically most zones



### ***Future Study Areas***

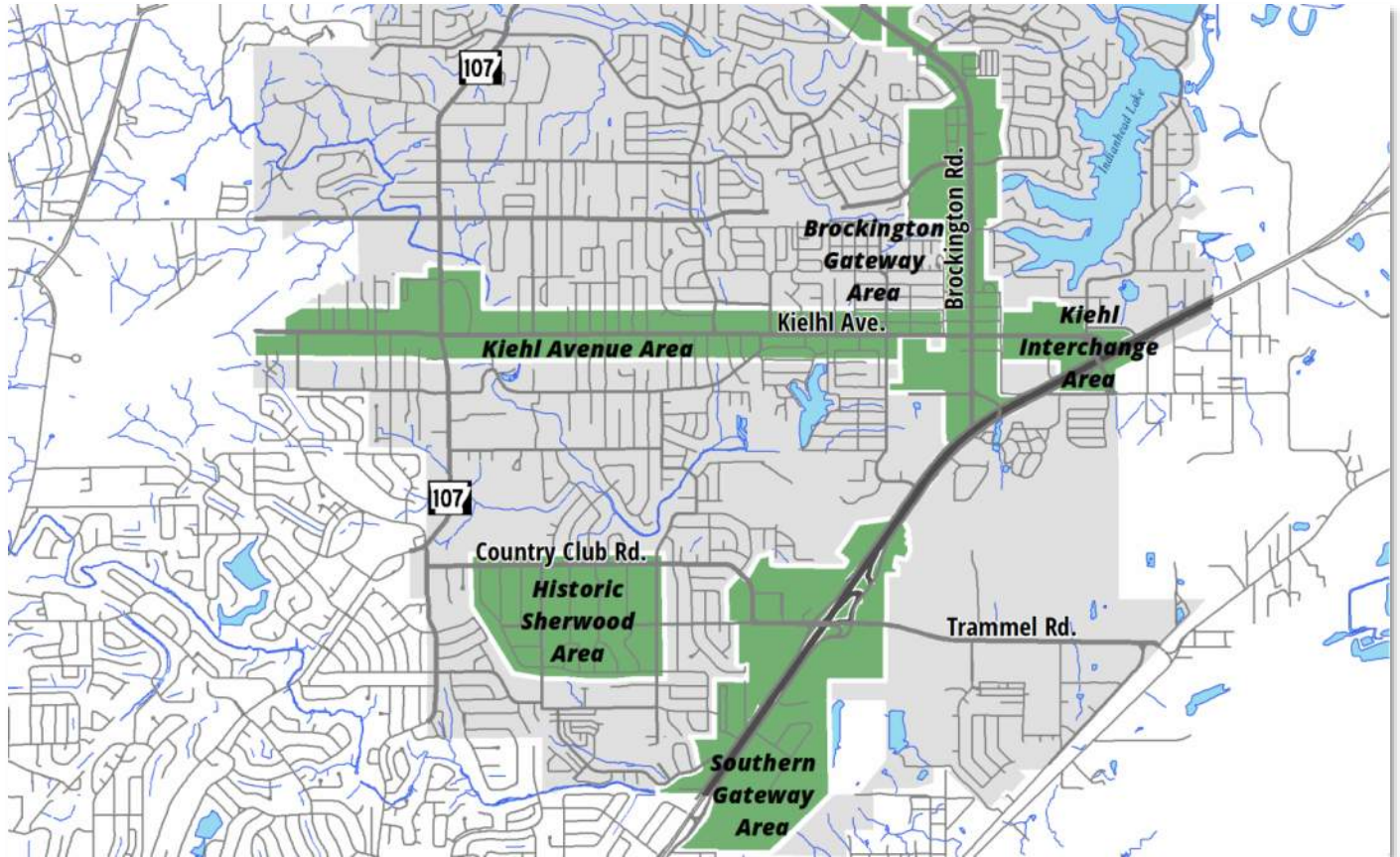
Understanding that fine-grained study of special areas in the city may be necessary, several areas in the city were identified to be examined by small-area studies in the future. These are areas with land use patterns or development form that could or should change in the future with development/redevelopment. These also indicate many areas with special significance for the community.

#### *North Sherwood*



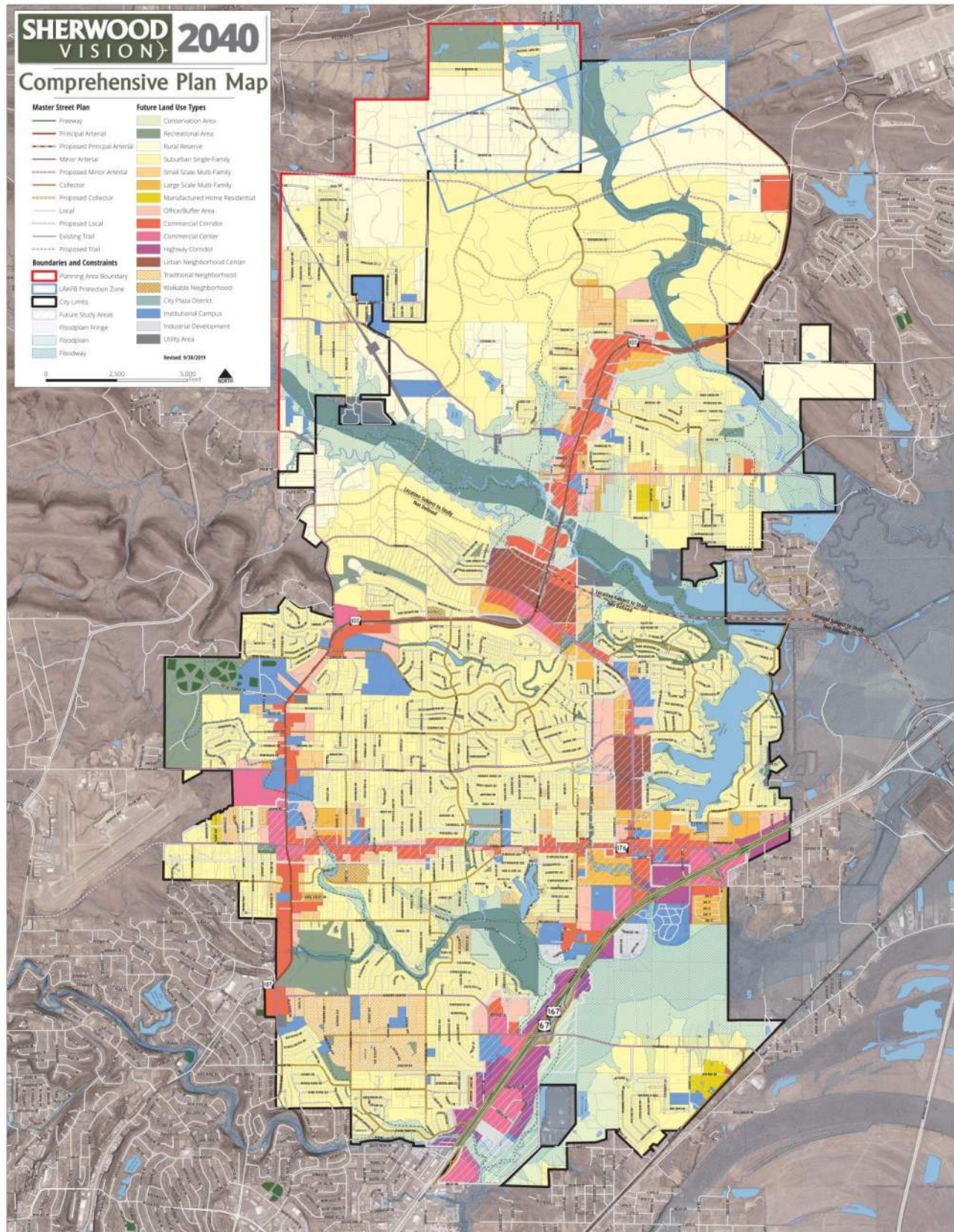


South Sherwood





*Place Type Plan Map*

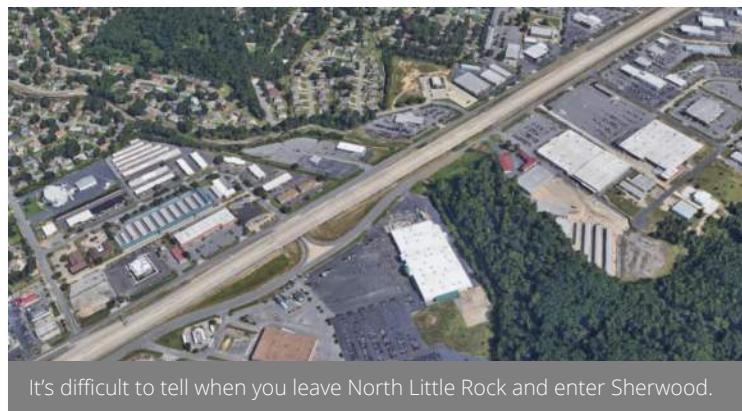




## 4.2: Community Identity and Image

While Sherwood has a rich and unique heritage from the early days of the Kellogg mines to Mr. Matthews' Sylvan Hills Country Club, this heritage is not readily apparent in the community's built environment. Expressed another way, the city has little development that exudes its identity and history, fostering a community character that is indistinguishable from other suburban communities across the country.

This lack of identity is due in large part to the fact that Sherwood has no focal point or town center and no discernible boundary between it and neighboring communities. While many Arkansas cities were established in the 1800s and developed a downtown, Sherwood's incorporation in 1948 meant much of the city has developed in the era of the suburb. As such, the city, much like any community, has developed in a manner responsive to market demand at the time. However, market demands are shifting which could leave Sherwood in a vulnerable position relative to other communities.



With the shift to online shopping, suburban commercial strip centers and big box stores are encountering stress. Entertainment, dining, and experiential retail are becoming more important. Downtowns are increasingly becoming resurgent because of their uniqueness and presence of buildings that are readily adaptable entertainment venues, restaurants, bars, and small-scale retail. Developers have responded by increasingly building "town center" style developments which feature a mix of apartments/condos, retail, dining, and entertainment. Responding to changing market demands and addressing issues of community identity are important elements to this plan.

### Sherwood Community Identity and Image in 2040?

What does this mean for Sherwood's community identity and image 2040? Here are some items the plan should address:

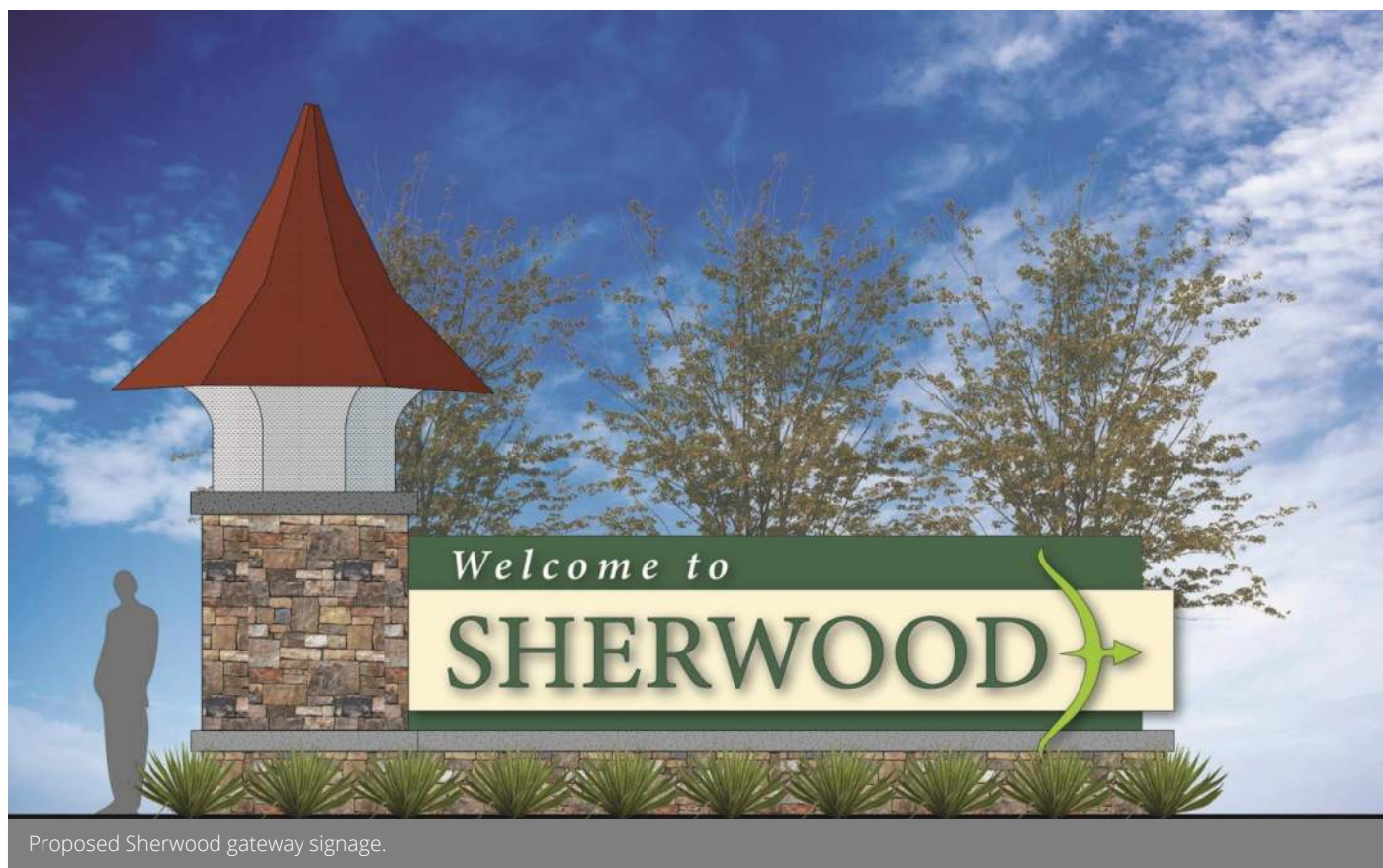
- *Sherwood Historic District* – While much of Sherwood developed during the era of the suburb, the "Old Sherwood" neighborhood near the Greens at North Hills (originally Sylvan Hills Country Club) has a good deal of historic character. Its small narrow lots, historic homes and cottages, and neighborhood parks represent one of the most unique parts of Sherwood's history. Justin Matthews and his company Metropolitan Realty and Development built the neighborhood, and later built much of Sherwood. The city should celebrate this heritage by creating a special district for the area that encourages preservation of its historic homes and compatible infill development that fits the character of the area.
- *Update Development Regulations* – Sherwood's current land use regulations are confusing and often contradictory. While city staff strive to be friendly to development, the city's land use regulations often present challenges that make the development process unnecessarily difficult. The city should work to overhaul its land





use regulations in a way that aligns with the goals and policies of this plan. This includes ensuring those regulations result in development that reflects the plan's vision for Sherwood and reflects a unique community identity. This effort will require a complete rework of the city's Zoning Code and Subdivision Code. Doing so will help achieve better development outcomes and foster a clearer development process for everyone involved.

- *Creating Places not Developments* – Modern development practices focus on the creation of residential subdivisions devoted exclusively to single-family uses and often devoid of amenities. As a result, these developments often have a 40-50-year shelf-life before severely suffering from disinvestment. Development that creates places, fosters community and encourage continual reinvestment often due to the amenity created by the development. The city should encourage development that creates amenity. This can be achieved through mixing land uses and densities to resemble historic patterns of development, or by integrating amenities such as parks or greenspace that is held in common ownership by the residents of the development.
- *City Gateways* – Because the boundary between Sherwood and North Little Rock is difficult to discern, many key Sherwood amenities are often confused as being in North Little Rock. The city should consider establishing clear, monumental gateways that clearly signal one is entering Sherwood. This can be achieved through gateway signage as well as through development patterns. The city should explore, in addition to new gateway signs, creating a special overlay district along the U.S. Highway 67/167 corridor as well as near the Brockington Road Interchange.



- Sherwood Town Center* – Entertainment options in Sherwood are far and few between. Additionally, the city has considerable retail leakage to Little Rock and North Little Rock. The Brockington Road and Highway 107 interchange presents the city a unique opportunity. Within the 30 years, the area will be the “center” Sherwood, as the city grows northward. The area could serve as an excellent location for the type of town center style development previously mentioned. The area could serve as a focal point for the community, being a gathering point and heart of the community. The city should consider actively working to attract a developer to construct a town center style development near the Brockington Road and Highway 107 intersection. Below is a conceptual example of such style development.





- *City Civic Center* – In many respects, a civic center for Sherwood already exists. The convergence of the Harmon Recreation Center, Oakbrooke Elementary, Senior Center, Police Department, Thornhill Park and Pool, and City Hall from most of the elements of a functional city civic center. However, what is lacking is a square around which all the elements are placed. The conceptual sketch below shows how an enhanced city civic center might look. It would provide a central gathering place for community celebrations or festivals, and represent the place that the community as whole identifies as “Sherwood”.



- *Sherwood Entertainment District* – Entertainment Districts are implemented as a way for cities to broaden their array of community attractions to its citizens, while in many instances enabling redevelopment of underutilized properties. This holds true for a particular area situated on the western side of Sherwood. Specifically, the City of Sherwood adopted legislation during the summer of 2019 that established an ‘Entertainment District’ for a defined area in and around the intersection of North Hills Blvd. and Country Club Road (Ord. No. 2265). The ordinance is part of a larger effort for the city to encourage quality ‘placemaking’. Placemaking is a term used by land planners to describe uses that help to define a community, and to draw people into the city. The city should explore innovative land use and zoning strategies to encourage systematic, planned redevelopment for entertainment, civic, and cultural uses in this area. Moreover, Planning Unit Development (PUD) land use, and underlying zoning, should be placed strategically in this area to allow for a more flexible set of development rules, which can be more effective in bringing innovative land use projects from concept to buildable design.

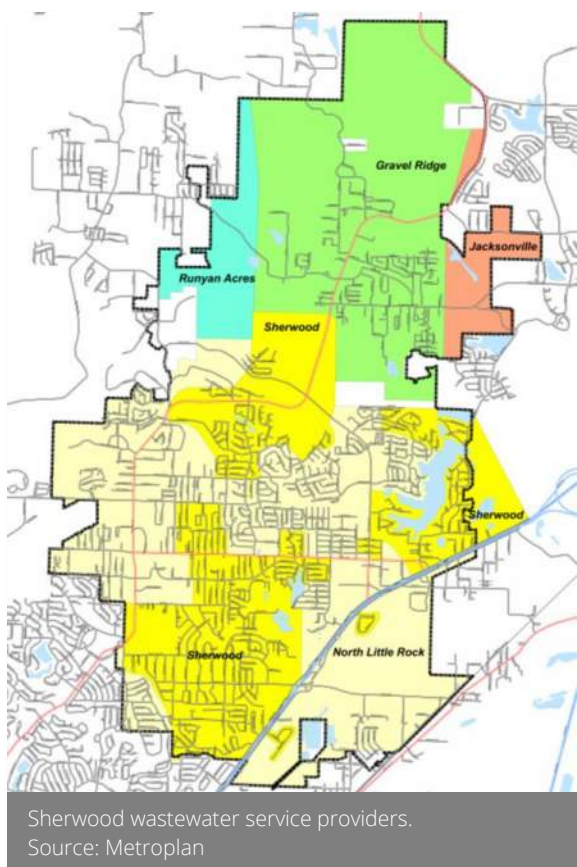
### 4.3: Growth Management and Fiscal Health

#### **Utility Infrastructure and Emergency Services**

The utility systems serving Sherwood residents will have a significant role in guiding and enabling the city's growth. Sherwood is extremely unique in that its city limits and planning area are served by several utility providers, with electric, wastewater, and water all being served by multiple providers. The city has four wastewater providers alone: Sherwood, North Little Rock, Gravel Ridge, Runyan Acres, and Jacksonville. The availability of wastewater service is often determinative of whether land can develop at urban or suburban densities. This can potentially place the city in a difficult position.

The lack of control of sewer utilities over large parts of the city means Sherwood does not have the political or financial control to ensure utility systems are properly upgraded or maintained to guarantee growth can continue in the city. If issues arise with one of the city's utility providers, the city has few tools to compel the provider into action. This could potentially halt growth.

*Sherwood's fragmented system of utility providers could pose risks to the long-term growth of the community and should be a top priority to address.*



Sherwood faces a similar issue with regards to its fire services. The city is served by the Sherwood, Gravel Ridge, and Runyan Acres fire districts. This type of fragmentation often results in inequity in fire response as well as ISO ratings. It may also mean redundancy of fire service infrastructure due to district overlaps. The city should explore the consolidation of all fire districts into one. Additionally, conversion to a city fire department as opposed to the area's current fire district governance should be examined.

#### Sherwood Utility Infrastructure and Emergency Services in 2040?

What does this mean for Sherwood's utility infrastructure in 2040? Here are action items to address:

- *Explore Wastewater Partnerships/Consolidation* – The city should carefully study and explore the possibilities of combining or enmeshing its wastewater utility with that of Gravel Ridge. Much of Sherwood's growth areas lie to the north where Gravel Ridge provides service. The city should also study and explore possibilities of creating a regional sewer utility with North Little Rock due to the degree to which the utilities are enmeshed. However, any discussion should be frank, and any merger should ensure Sherwood's city interests are protected.
- *Fire District Consolidation* – Sherwood should explore options to consolidate the Sherwood, Gravel Ridge, and Runyan Acres Fire Districts.

One district or city fire department could greatly save on the cost of buildings, trucks, and equipment necessary for each district to function through an economy of scale and reduction of redundant equipment. Additionally, consolidation could help ensure equitable fire response times and ISO ratings exists for all city residents, bridging the divide between Sherwood and Gravel Ridge communities.

## ***Stormwater Management***

Though often ignored, proper stormwater management is an important component to guiding future urban growth. Communities often do not take the issue seriously until drainage problems create localized flooding or degrade water quality in nearby streams, rivers, and municipal water supplies. Localized flooding generally occurs due to irresponsible land development of commercial sites and residential subdivisions. Poor development practices disrupt the natural hydrological functions of land by pushing stormwater off a property more quickly and in greater quantities. If these irresponsible development practices become common place, the problems only magnify as more water is quickly pushed off more and more properties creating greater flood hazards.

*A stormwater utility would provide ongoing funding to allow the city to maintain, build, and repair drainage infrastructure across the city.*

Proper development practices seek to slow the run-off of stormwater by either retaining or detaining it on-site. This prevents flooding from excess run-off at drainage choke points downstream. The city should ensure responsible development practices occur and make investments to remediate current issues.

### *Sherwood Stormwater Management in 2040?*

What does this mean for Sherwood's stormwater system in 2040? Here are action items to address:

- *Explore Creation of a Stormwater Utility* – As such, the city should prudently examine the creation of a stormwater utility. Such utilities are often funded by fees assessed with utility bills. Often the fees are tied to site size or impervious surface of a site. As such, business and residents both fund the utility. While potentially unpopular, a utility could save money and heartache for both the city and its residents in the long run.

## ***Annexation***

Municipalities in Arkansas may annex adjacent land by one of three methods. The first method is by direct election determined by a majority of the qualified electors voting on the issue, i.e. the voters of the municipality and the voters of the area proposed for annexation. Section 14-40-302 of the Arkansas Code, Annotated, sets forth the authority.

The second method of annexation deals with land surrounded by a city's boundaries. These "islands" may be annexed by the passage of an ordinance calling for the annexation of the surrounded land in accordance with A.C.A. § 14-40-501 et. seq.

The third method of annexation is by the petition of a majority of the real estate owners of any part of a county contiguous to and adjoining any part of the city. The process must conform to the provisions set forth in A.C.A. § 14-40-601 et. seq.

Of these methods, annexation by petition is the most common and generally the least controversial. Continued reliance on this method, however, tends to produce irregular corporate limits. Such irregularity can, in turn, cause problems providing public safety and other municipal services. A common misunderstanding is that annexation by petition is done by one property owner. In reality, a majority of owners of the majority of a given area may secure a petition for annexation without the agreement of other property owners in the given area.

Annexation by election allows the municipality to select the configuration it feels is suitable for urbanization. It gives the city the greatest control in deciding which lands should be annexed for which the city can provide effective and



efficient delivery of public services. Furthermore, strategic use of this annexation method enables the city to grow in a rational manner in terms of land area.

The city should carefully examine all annexation requests using cost-benefit analyses, and only accept those that may strategically benefit the city. These analyses should examine both those economic considerations that can be measured and political considerations which cannot be as easily measured. Such examinations should provide cost outlays for additional city service provision and/or potential sales tax revenue increases as well as acknowledge the long-term political ramifications for an annexation. Using these types of analyses would allow the city to see the impacts of annexation in a more transparent way.

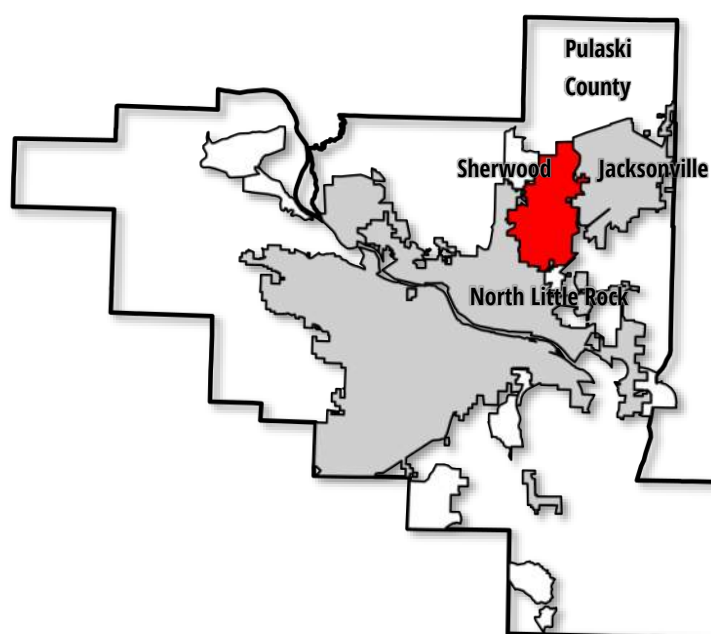
*Sherwood has nearly 4,000 acres of vacant land in its city limits. This land supply should provide the city plenty of room to grow well into the future.*

There is enough vacant land in Sherwood with nearly 4,000 acres with future development potential to accommodate all future growth in the next 20 years. Sherwood's proactive step in annexing the Gravel Ridge area of North Sherwood in 2008 ensured the city will have room to grow for decades to come.

#### Sherwood Annexation in 2040?

What does this mean for Sherwood's annexation heading toward 2040? Here are action items to address:

- *Annexation Plan* – Sherwood leaders should develop a plan that identifies areas that may be advantageous for future annexation. This plan should be built upon careful analysis of the benefits and costs that annexation of these areas may present. Can the areas be served by city utilities? What are the conditions of existing infrastructure? Will the city be responsible for immediate fixes to infrastructure? Do the areas have potential for commercial land use?
- *Annexation Analysis* – Voluntary annexation requests should be carefully scrutinized to ensure annex benefits both the city and the property owner. Annexations that will cause the city to incur large liabilities in the form of infrastructure repair or capital outlays for service provision should only be accepted if and when such annexations provide long-term benefits to Sherwood residents and taxpayers.



#### 4.4: Quality of Life and Recreation

Quality of life amenities are critical to community success. Things like parks, recreational programs, trails, and schools form a backbone for a quality of life that attracts and retains residents. This is in large part because proximity to a job is often a secondary concern in deciding where to live. This places Sherwood in a position where it competes against communities throughout the Central Arkansas region for new residents.

*Having a high quality of life is critical to attracting and retaining new residents and employers in Sherwood.*

The city presently has an excellent system of parks of which it should be proud. These range from small neighborhood parks such as Delmont and Henson park to regional amenities like Sherwood Forest and the Harmon Recreation Center. However, not all parts of the community share in the same level of recreational resources. Much of North Sherwood, having only been annexed within the last 11 years, lacks the types of parks facilities found in the south part of the community. Also, parks like Sherwood Forest still have considerable room to full development.

The creek at Henson Park.



Many creeks and streams flow through Sherwood. The land alongside these creeks and streams is often not developable. As such, the small ribbons of land along creeks provide excellent opportunities to create greenways that create natural landscape connections throughout the community. These greenway often present good locations for trails networks. In some locations, these trail networks already exist in the community, other opportunities are, as of yet, unrealized.

Another critical component to quality of life are schools. Schools are highly determinative of where people choose to live within a metropolitan area. Presently, schools in Sherwood are not locally controlled, but controlled by a larger school district encompassing much of Pulaski County. This means the community is limited in its ability to determine how the local schools are ran, how resources are allocated, and where/if new schools are built. This lack of control can serve as impediment to the community and discourage potential residents from locating in the city.

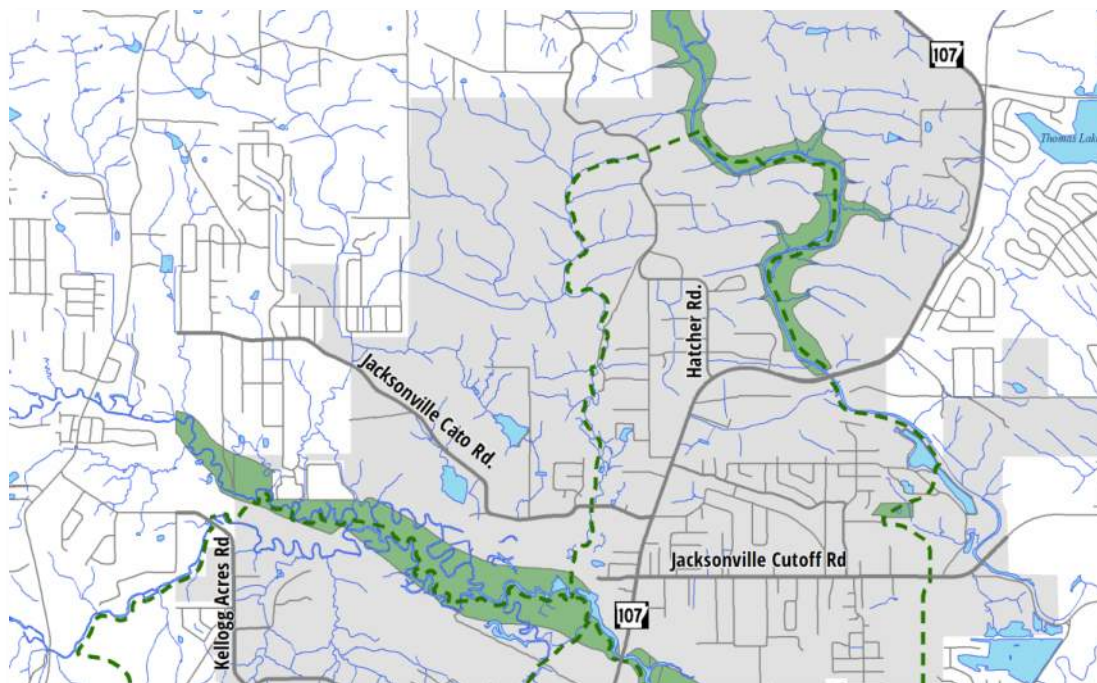
#### Sherwood Quality of Life in 2040?

What does this mean for Sherwood's quality of life in 2040? Here are action items to address:

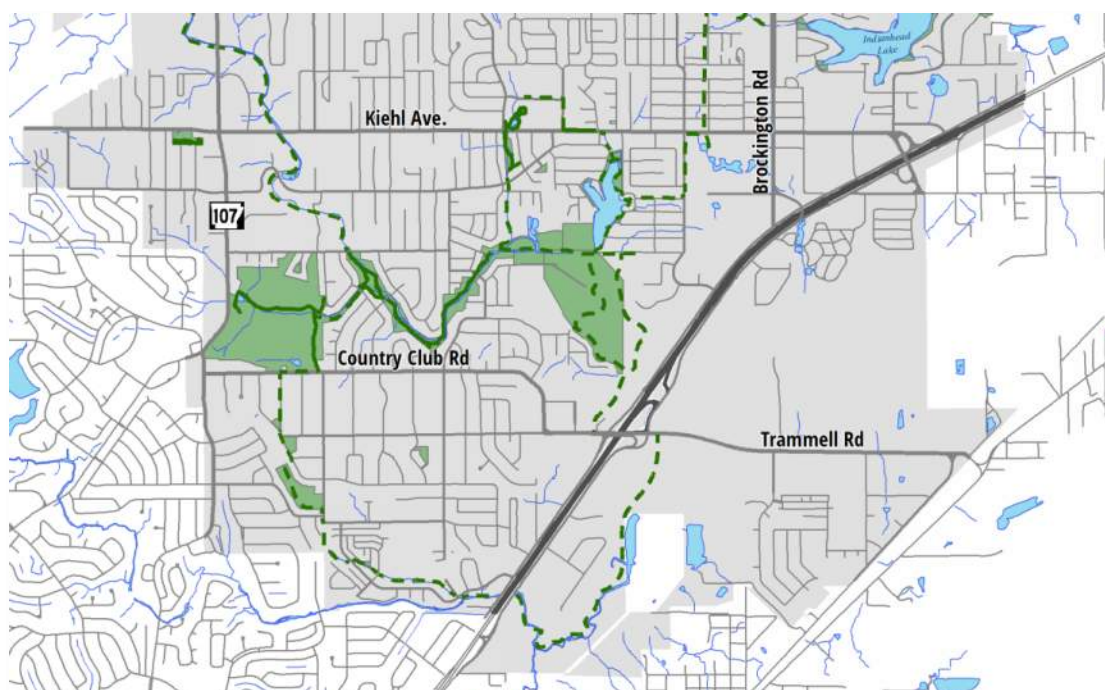
- *Northside Regional Park* – The lack of park amenities in North Sherwood presents an equity for city residents in that part of the community. While much of Sherwood has developed with small community parks, that model of park service delivery is costly and seldom pursued at present. Large regional parks allow for easier upkeep and maintenance as crews and equipment as consolidated in one location. The city should explore the development of large regional park in North Sherwood to develop amenities for that part of the community as well as the city as whole. The scale of a regional park allows for unique amenities not often seen in smaller parks such a disc golf, expansive open spaces, or outdoor water park facilities. A potential location for a park could be along Kellogg Creek.

- *Trail System* – Trails are a community amenity that are increasingly being expected in successful cities. Northwest Arkansas has demonstrated the tangible economic, health, recreation, and tourism benefits of having a robust trails system. Sherwood is ideally laid out in a way to develop an interconnected system of greenway trails. The city should consider committing resources to greater development of its trails system. Below are maps indicating potential routes. These routes are also indicated on the Comprehensive Plan Map.

North Sherwood

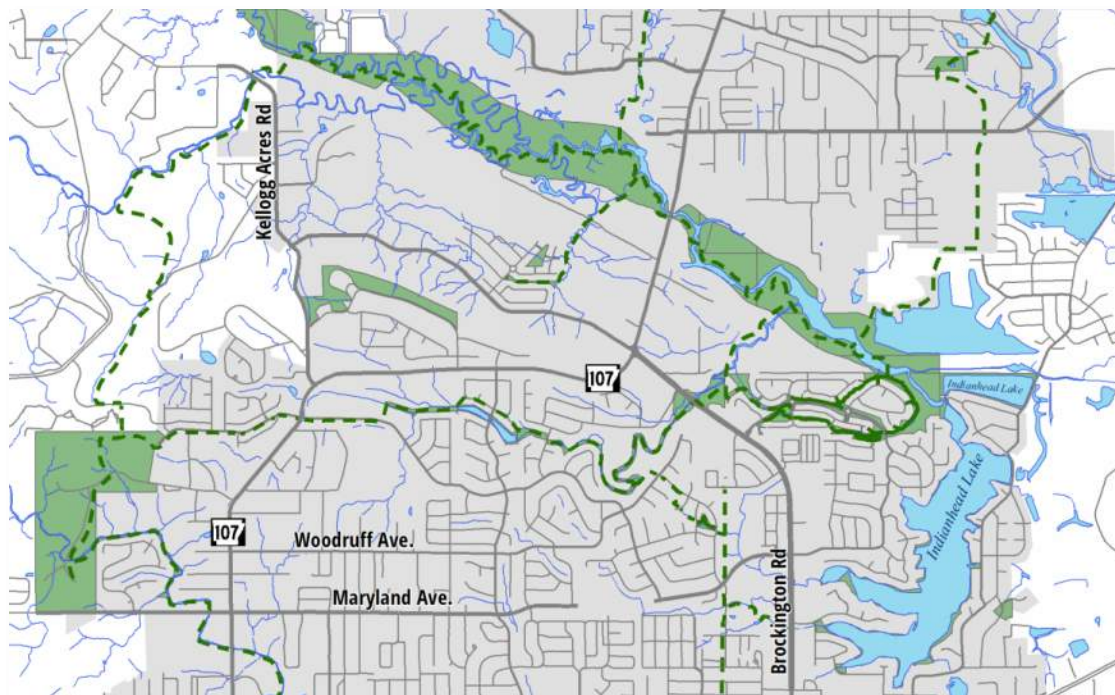


South Sherwood





- *Kellogg Creek Blueway* – Kellogg Creek is a unique amenity. It divides the community, but also presents a diverse and rich aquatic wetland environment not seen in many cities. It also presents an excellent opportunity for a blueway canoe trail in an urban location. Utilization of the area for a blueway canoe trail requires few resources except in the form of making the creek accessible to canoers. The city should explore creation of the blueway with the addition of trails for bike and pedestrian access. Below is a map showing the creek and potential trail.



- *Sherwood School District* – Local control of schools in the city is an important tool for allowing the community to determine its own future success or failure. Without local control the community is limited in its ability to determine how the local schools are ran, how resources are allocated, and where/if new schools are built. The plan supports the creation of an independent Sherwood School District based around the boundaries of the Sylvan Hills High School attendance zone. Creation of this district is also vitally important to helping Sherwood solidify its community identity.

This expansion at Sylvan Hills High School will bring the campus capacity to 2,200 students.



## 4.5: Transportation and Mobility

### ***Transportation Issues***

Transportation and personal mobility have become a growing concern for Sherwood, as the city has nearly doubled in population over the last 30 years and continues to see significant development pressure. The city's growth to the north and its position as a commuter community have exacerbated these issues. As Sherwood continues to grow northward, major transportation investments will be needed to adequately tie together the commuter and ensure easy access across the city and major job destinations south of the city.

Though Sherwood possess three interchanges on U.S. Highway 67/167, much of the city's north-south traffic is filtered through the Brockington Road corridor. This issue will only become worse over time as the near the Highway 107/Brockington/Oakdale intersection continues to develop. Additional access to U.S. Highway 67/167 will likely be needed.

*It's likely Sherwood will see substantial need for major transportation infrastructure investments to ensure roadways in south Sherwood aren't overwhelmed due to growth in North Sherwood.*

When Sherwood annexed the Gravel Ridge area, it brought in a community that had little planning to direct its growth. As a result, the area's transportation infrastructure is limited with essentially all traffic in the area funneling onto Highway 107 to the north and south, and Jacksonville Cutoff Road to the east. The area has no good east-west arterial connection, and the area is only connected to the rest of Sherwood by a single bridge over Kellogg Creek. Major investments in North Sherwood will be necessary to ensure continued and well-planned community growth.

### ***Sherwood Transportation in 2040?***

What does this mean for Sherwood's utility infrastructure in 2040? Here are action items to address:

- *North Pulaski East-West Connector* – The Northbelt Freeway project died for numerous reasons after fits and starts for years. Some right-of-way for the facility still exists. The city should carefully study and explore the possibilities of reinventing the dead project as an arterial connection for the community. It could provide a much-needed additional access to U.S. Highway 67/167 from North Sherwood, but additionally could encourage more development near the intersection of Brockington and Highway 107. However, this project should be examined carefully and from a regional perspective. For the project to be remotely feasible as an arterial road, it must be approached as a regional project.
- *North Sherwood Street Network* – To ensure North Sherwood develops in a manner that creates a more cohesive, unified Sherwood, major street network investments will be needed in the areas north of Kellogg Creek. The city should explore more thoroughly proper routing for a new crossing of Kellogg Creek. One alternative is a new collector street extending from Oakdale Rd. to a proposed western extension of Arnold Dr. Regardless, a robust street network will be necessary to help ensure that Highway 107 does become more a traffic funnel than it already is.
- *Connectivity/Collector Network* – Connectivity is key to help distribute traffic throughout a street network. In many areas of Sherwood, the city has connectivity issues, making certain routes circuitous or causing traffic to funnel onto arterial roads. The city should continue to focus on ensuring its network of collector streets connect either through development proposals or through city construction projects. Examples include the Hemphill Road extension, the recent Oakbrooke Drive connection, and the Maryland Avenue extension.

## ***Master Street Plan***

### Arterials

This plan suggests access management of arterial roads throughout the planning area as a means to preserve roadway capacity and forestall future street widening. Many of the arterial roads within the planning area are state highways. Access management of these roads will likely require access management agreements with the Arkansas Department of Transportation and Metroplan.

The primary function of an arterial is to move high-volume traffic. Ideally, this function would be protected. However, historic development patterns and economic factors sometimes lead to problems in maintaining high-volume traffic flow. Many of the city's arterial roads were initially constructed and subdivided in a manner that placed a priority on access, not moving traffic. As the city has grown, traffic along these primary corridors has increased dramatically and their roles have changed. This access-traffic flow conflict reduces their efficiency and capacity. Such issues cannot be easily or readily amended, and addressing them requires steady planning and dedicated, long-term implementation of access management standards and policies.

Economic factors can also play a role in determining the long-term efficiency and capacity of arterial roads. The high traffic volumes on arterial connectors attract commercial development that desires a great degree of property access. These development demands can easily result in arterial roads littered with curb cut after curb cuts and greatly diminished capacity and traffic flow. Because cities in Arkansas depend heavily upon sales tax revenue, the Planning Commission faces a constant need to balance traffic concerns with economic development concerns. Finding that balance will be important to ensuring economic growth and protecting taxpayers. Methods of achieving this balance include access management. The plan does not propose construction of new arterial road facilities.

### Collectors

It is typical to design collectors so they will not function as continuous through streets, but serve to collect traffic and empty onto adjacent arterial corridors. In a grid-street pattern, a street several miles long may serve as a collector rather than an arterial if its predominant use is only to reach the next junction with an arterial. This improved connectivity allows the transportation system to be less dependent on large arterial roads to move traffic. Examples of this kind of network can frequently be seen within the older portions of many cities.

The policies and proposals of this plan support a street network that uses collector streets to improve connectivity. The City will strive for a system of collector streets spaced approximately one-quarter to half a mile in both north-south and east-west directions. In most cases there are existing streets or extensions of existing streets. In undeveloped areas, they are indicated on the Plan Map as general locations. As new developments occur, developers will be responsible for construction of the collector street system. This will include improving all, or a portion of, existing streets located within or adjacent to the developments.

Some streets designated as collectors are fully developed in a manner that will preclude their being brought into compliance with the standards adopted. These are maintained as collectors on the plan for two reasons. First their designation as collectors may result in avoiding any further degradation to their functional classification. Second, if major redevelopment does occur in the future, the Planning Commission may, at that point require that such redevelopment adhere to the provisions of this plan.

## South Sherwood



### *Project: Fairway Connection (1)*

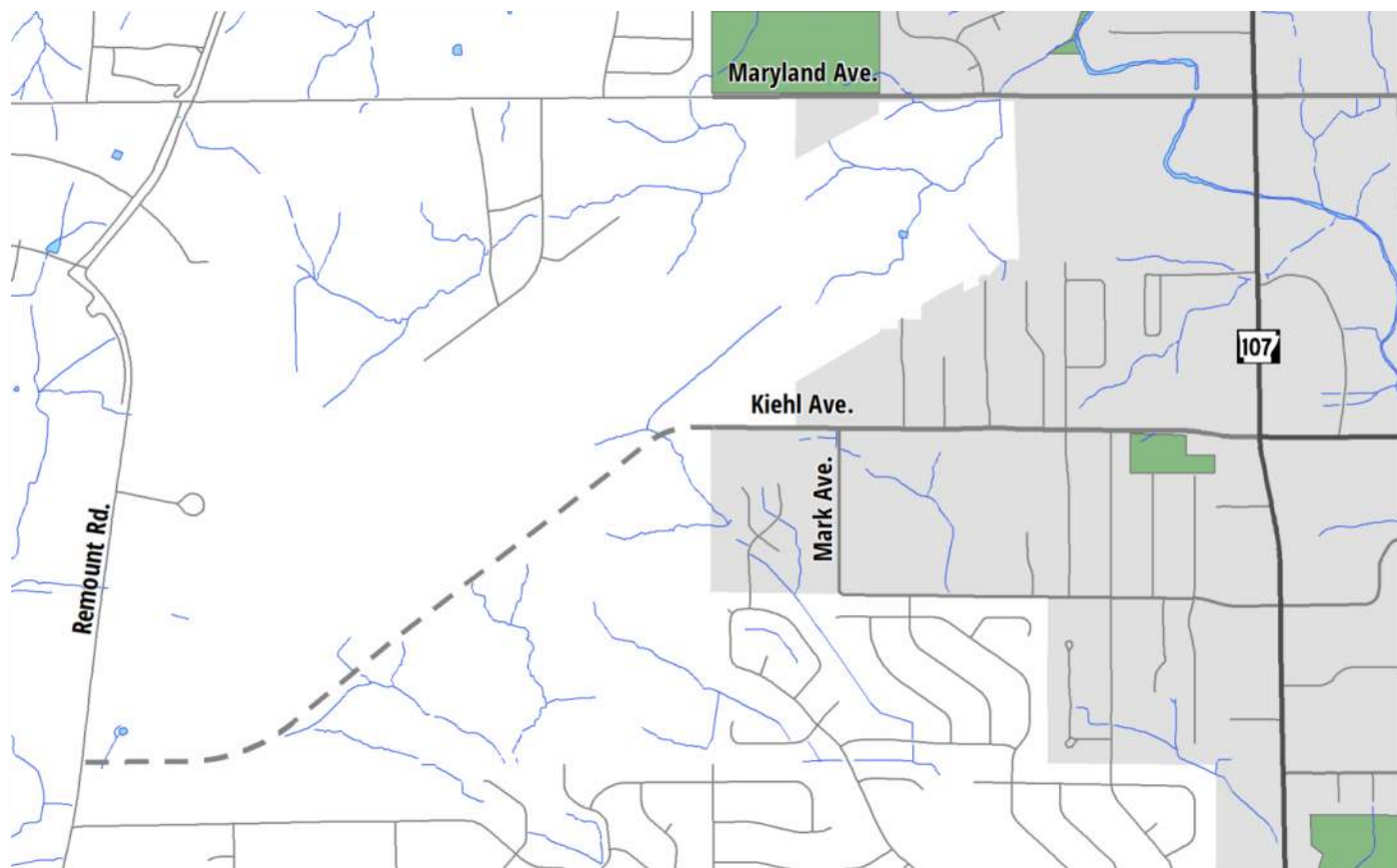
*Purpose:* The failed connection of Fairway Ave. in North Little Rock to Fairway Ave. in Sherwood was unfortunate. However, possibility of an alternate connection still exists without requiring the construction of a new bridge. The connection would extend Abercorn Pl to the southwest to connect to Fairway Cv. In North Little Rock. This project would require cooperation between the two cities but could provide a valuable alternative to North Hills Boulevard for south Sherwood residents.

### *Project: Claremont Extension (2)*

*Purpose:* Claremont Ave. could be an important north-south connector between Country Club Rd. and U.S. Highway 67/167. What is lacking is a small roadway extension to allow the roadway to provide a continuous connection between Silver Creek Dr. and Koehler Ave. This connection would aid in creating a more direct route to U.S. Highway 67/167 for south Sherwood residents.



## Kiehl Avenue

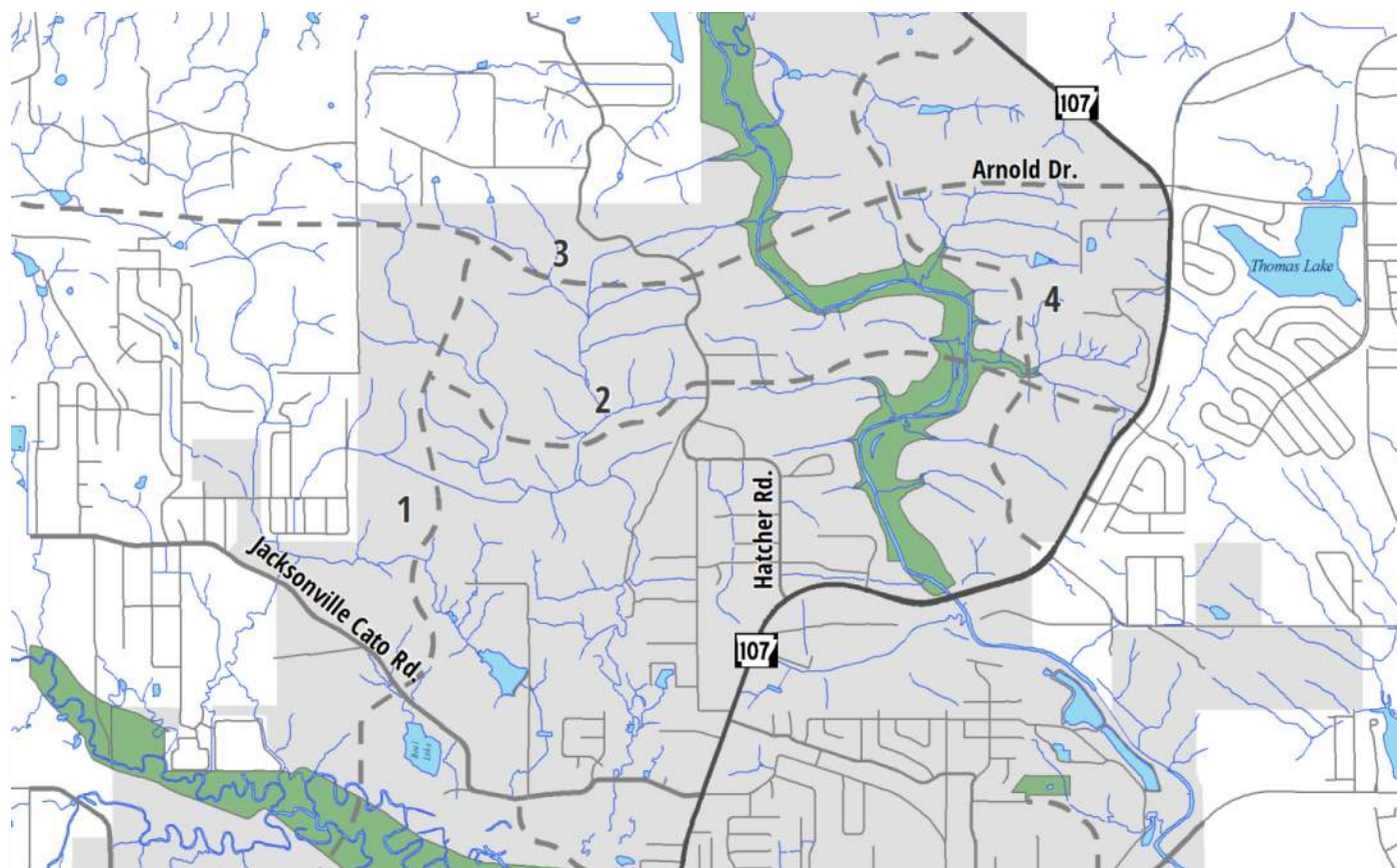


### *Project:* Kiehl Avenue Extension

*Purpose:* Kiehl Avenue currently dead ends on its western terminus at the city limits. This helps funnel more traffic onto Highway 107 south of Kiehl Ave. This connection would extend Kiehl Ave. west beside the North Little Rock Airport to connect to Remount Rd., which parallels Highway 107. It would alleviate a current dog-leg in Sherwood's street network that routes trips between Kiehl Ave. and Remount Rd. onto Highway 107 and Maryland Ave.

Note: This connection is represented on the North Little Rock Master Street Plan as an 80' ROW, three-lane street cross section.

Middle Sherwood



*Project: Kellogg Creek Collector (1)*

*Purpose:* This roadway would be an entirely new collector street either constructed by developers or the city. It would provide a crossing of Kellogg Creek from Oakdale Road north to a proposed extension of Arnold Drive.

*Project: Northside Collector (2)*

*Purpose:* This connection would connect the Kellogg Creek Collector to Highway 107 and provide for more orderly development of the area.

*Project: Arnold Drive Extension (3)*

*Purpose:* This road extension would provide a much needed east-west arterial for North Sherwood and potentially connect Highway 107 to Batesville Pike in addition to allow the area to more easily access Little Rock Air Force Base.

*Project: General Samuels Extension (4)*

*Purpose:* This connection would be built by developers as the general area is built out. Its primary purpose would be to ensure well-connected and orderly development occurs in the area.

## ***Cross Sections***

The following cross sections are provided to govern the construction of street and bicycle/pedestrian facilities by the City of Sherwood and through private resources by developers. These cross sections work in tandem with the City of Sherwood's Street Construction Standards that govern all aspects of roadway design and construction excluding street pavement width, curb and gutter requirements, as well as requirement of bike and pedestrian elements.

Cross Section Naming Convention	
<i>Roadway Class</i>	C1.0-3: Arterials, C2.0-6: Collectors, C3.0-5: Local Streets
<i>Bike/Pedestrian Elements</i>	I: Shared-Use Trails, II: Bike Lanes, III: Bike Lanes

### Arterials

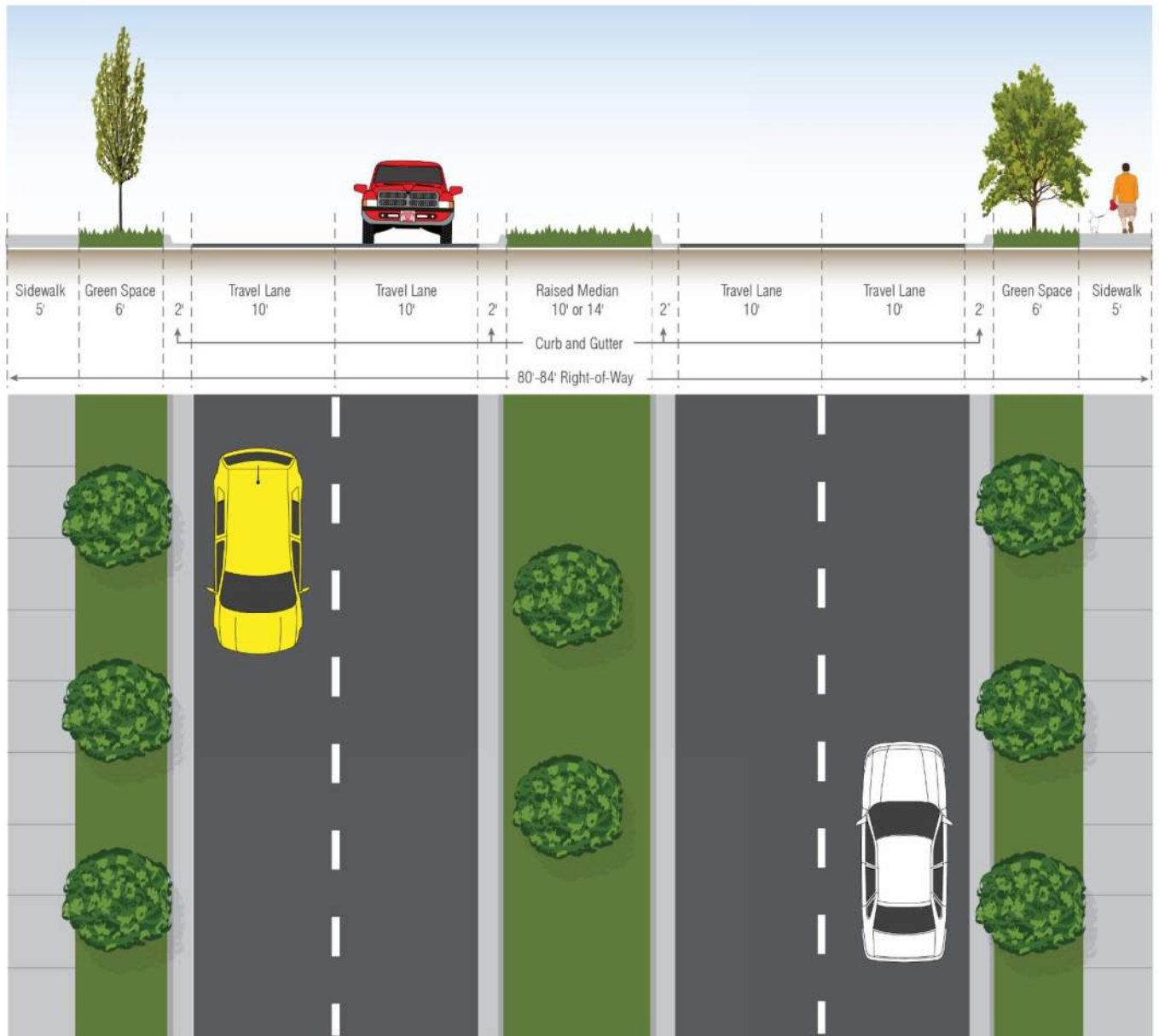
Arterials provide network connections within and through the urbanized area. These facilities typically provide a greater amount of access to adjoining land as compared to principal arterials, where the primary function is providing mobility by moving traffic. Principal Arterials are those corridors included within the Regional Arterial Network. Minor Arterials are those arterial corridors which are not included in the Regional Arterial Network. Design criteria is the same for both facility types.

1. Required Elements:
  - a) Right-of-Way: All required design elements must be included in the cross-section and located on publicly owned R.O.W. Sidewalks or bikeways may be located on permanent dedicated easements. The right-of-way must be sufficient to accommodate 4 lanes.
  - b) Curb and Gutter: Curb and gutter is required except in cases where terrain and/or forecast land use densities are compatible with an open shoulder design typically used in rural or exurban areas. The gutter width is not to be included in the travel lane.
  - c) Sidewalks: Sidewalks are required on both sides of the roadway. Minimum sidewalk width is 5 ft. and must be compatible with the Americans with Disabilities Act.
  - d) Green Space Buffers: A buffer is required between the back of curb and the sidewalk that is a minimum of 5 ft.
  - e) Pedestrian Crossings: Safe pedestrian crossing provisions are required to be demonstrated by the proposing jurisdiction or agency where more than 50 ft. of pavement (including the gutter) must be crossed by a pedestrian where pedestrian crossing is anticipated based on land use.
  - f) Bike Lanes/Trails: If on a planned bikeway route, the bicycle element must be included and must adhere to the bicycle design standards shown on the appropriate cross section. Where bike lanes are provided a minimum buffer from the main travel lanes is required.
  - g) Lane Width: 10 ft. minimum for main travel lanes or 11 ft. maximum lanes where the design speed and traffic mix warrant.

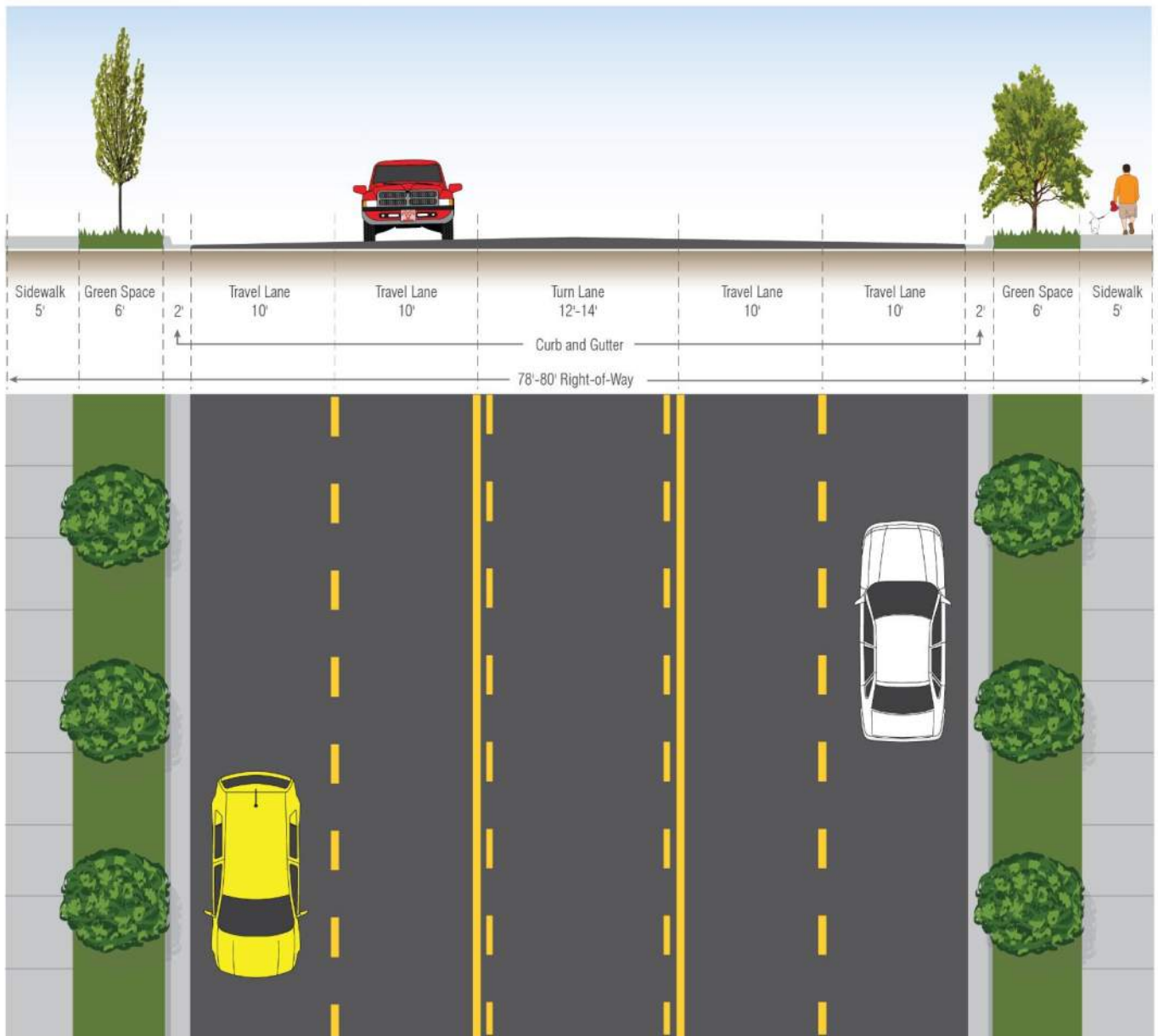
2. Optional Elements:
  - a) 8 ft. minimum paved shoulder on first phase of a planned 4-lane minor arterial, with or without curb and gutters.
3. Preferred Elements:
  - a) Landscaping of medians and buffers.
  - b) A non-traversable median is preferred for major retrofits and on new locations.
  - c) Where applicable, a multi-use trail is preferable over bike lanes.
4. Prohibited Elements:
  - a) Parking lanes.



C1.0 – Principal/Minor Arterial - Preferred

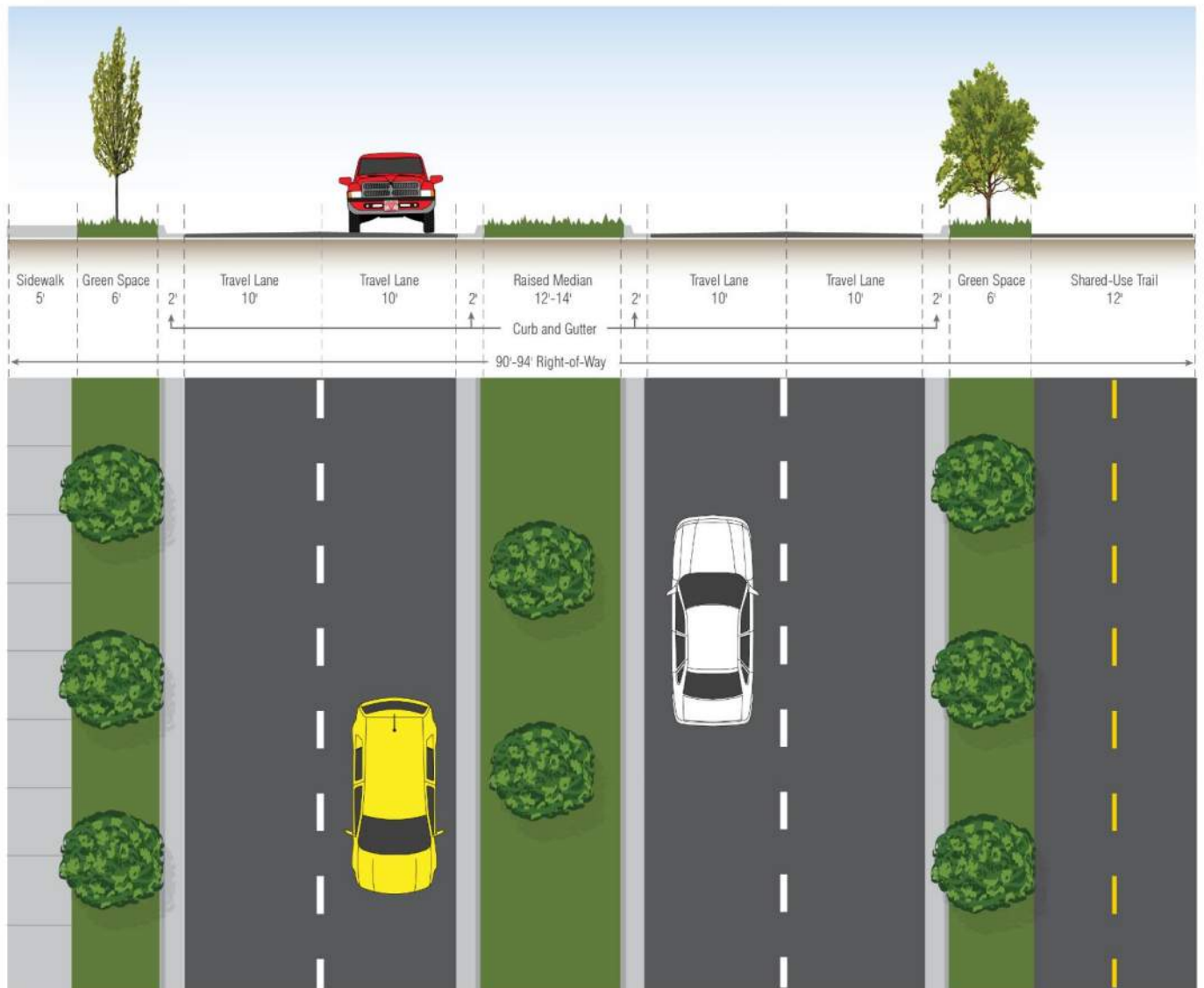


C1.1 – Principal/Minor Arterial - Alternative

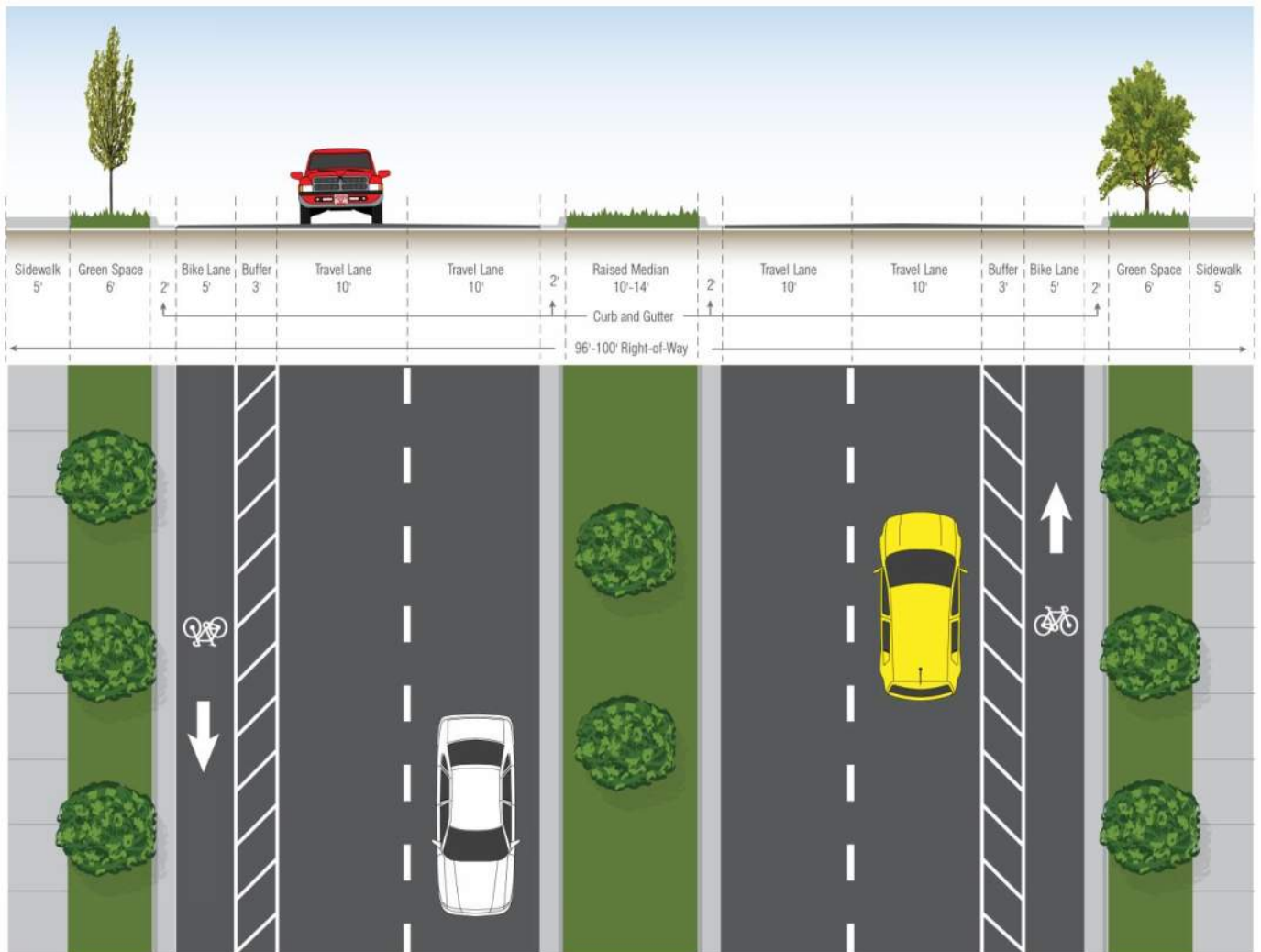


C1.2I – Principal/Minor Arterial with Trail

Median optional



C1.3II – Principal/Minor Arterial with Bike Lanes



## Collectors

Collector Roadways connect local traffic with the arterial roadway network and provide easy access to adjoining land.

### 1. Required Elements:

- a) Right-of-Way: All required design elements must be included in the cross-section and located on publicly owned R.O.W. Sidewalks or bikeways may be located on permanent dedicated easements.
- b) Curb and Gutter: Curb and gutter is required except in cases where terrain and/or forecast land use densities are compatible with an open shoulder design typically used in rural or exurban areas. The gutter width is not to be included in the travel lane.
- c) Sidewalks: Sidewalks are required on both sides of the roadway, except within Industrial Developments. Minimum sidewalk width is 5 ft. and must be compatible with the Americans with Disabilities Act.
- d) Green Space Buffers: A buffer is required between the back of curb and the sidewalk that is a minimum of 5 ft. However, no buffers are required where on-street parking is used.
- e) Pedestrian Crossings: Safe pedestrian crossing provisions are required to be demonstrated by the proposing jurisdiction or agency where more than 50 ft. of pavement (including the gutter) must be crossed by a pedestrian where pedestrian crossing is anticipated based on land use.
- f) Bike Lanes/Trails: If on a planned bikeway route, the bicycle element must be included and must adhere to the bicycle design standards shown on the appropriate cross section. Where bike lanes are provided a minimum buffer 1.5 ft. from the main travel lanes is required.
- g) Lane Width: 10 ft. minimum for main travel lanes or 11 ft. maximum lanes where the design speed and traffic mix warrant. There is a maximum of 2 travel lanes allowed.

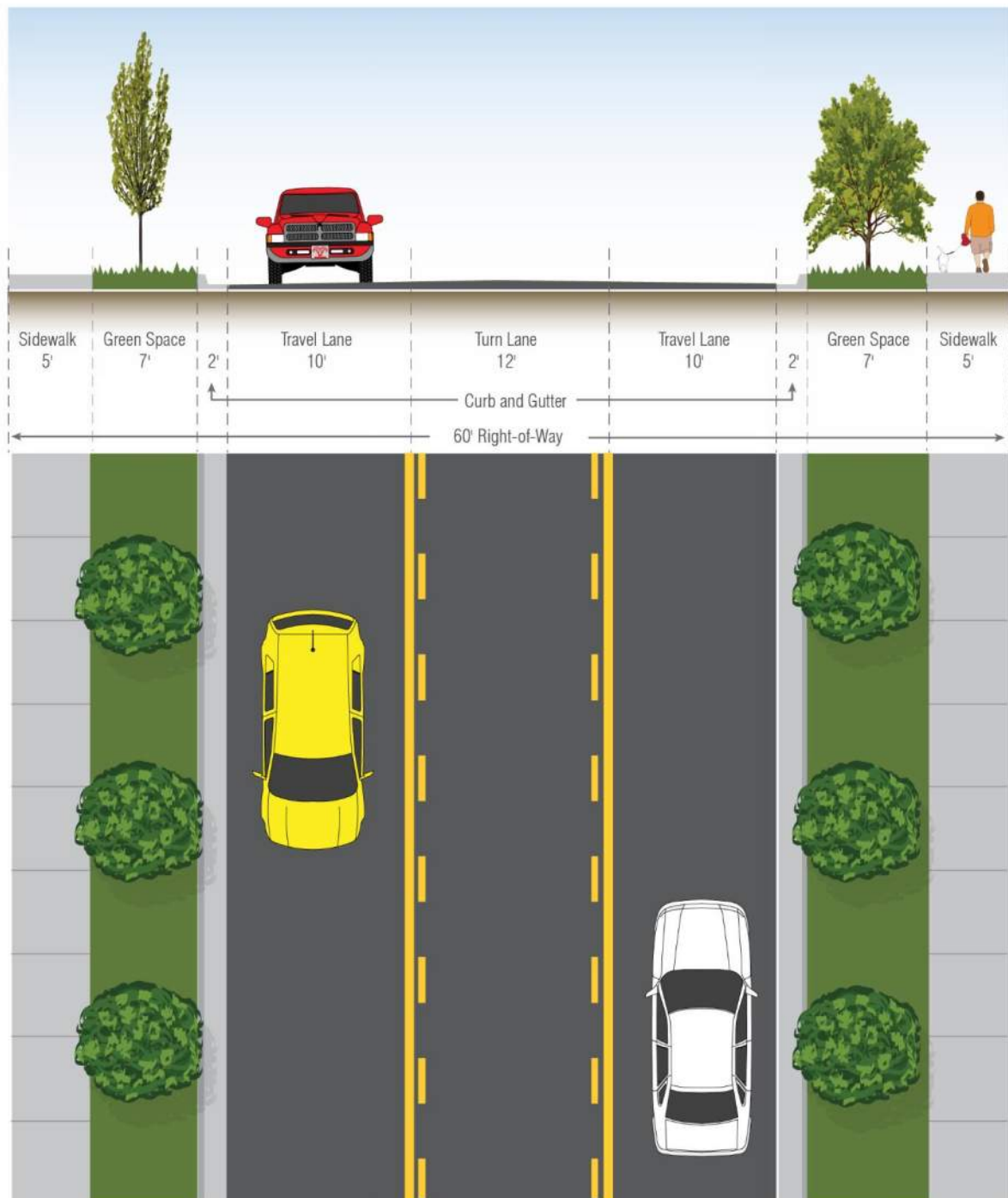
### 2. Optional Elements:

- a) Parallel parking may be used where warranted (C2.3, C2.4II).

### 3. Preferred Elements:

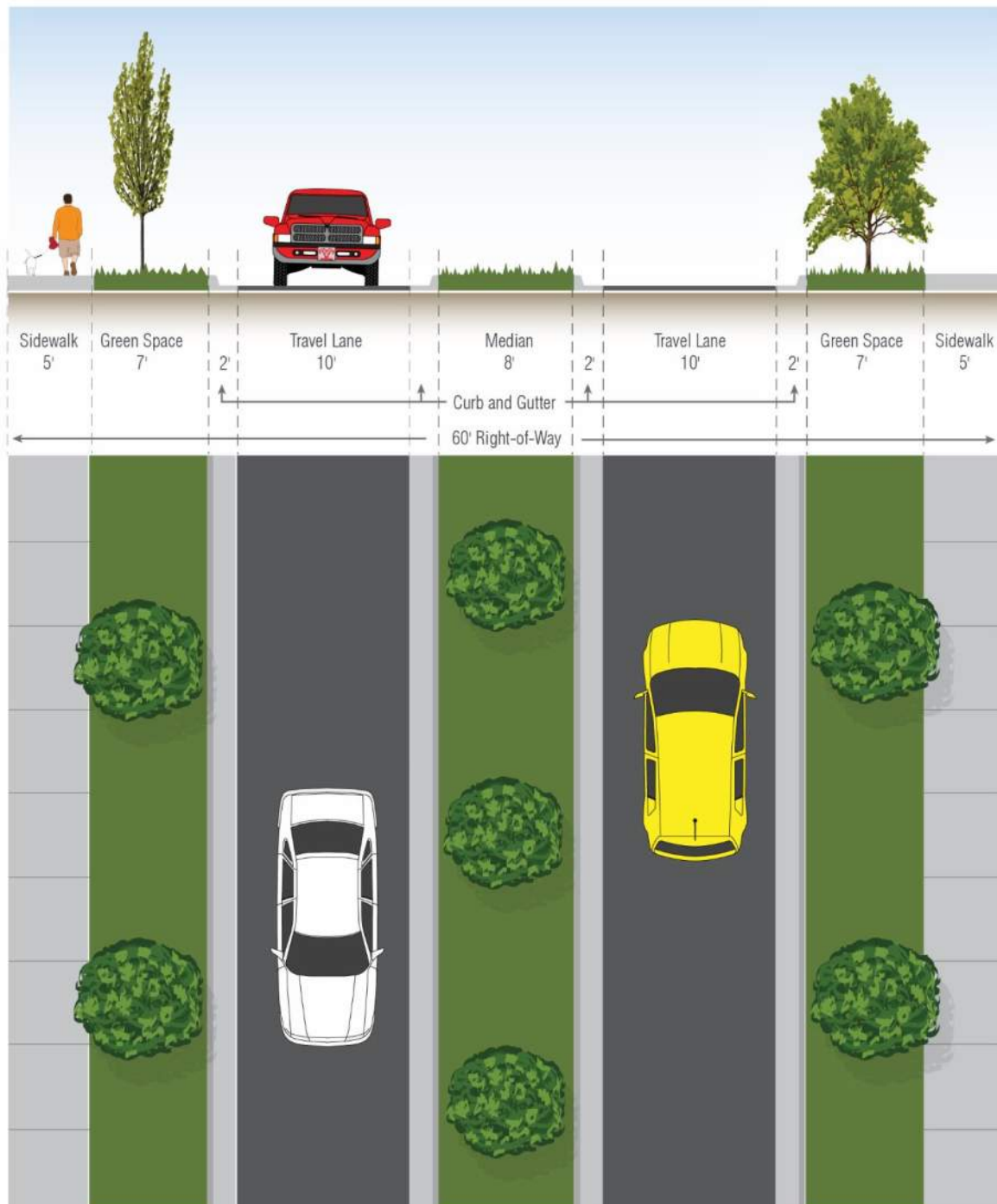
- a) Landscaping of medians and buffers.
- b) A non-traversable median is preferred for major retrofits and on new locations.
- c) Where applicable, a multi-use trail is preferable over bike lanes.

C2.0 – Collector

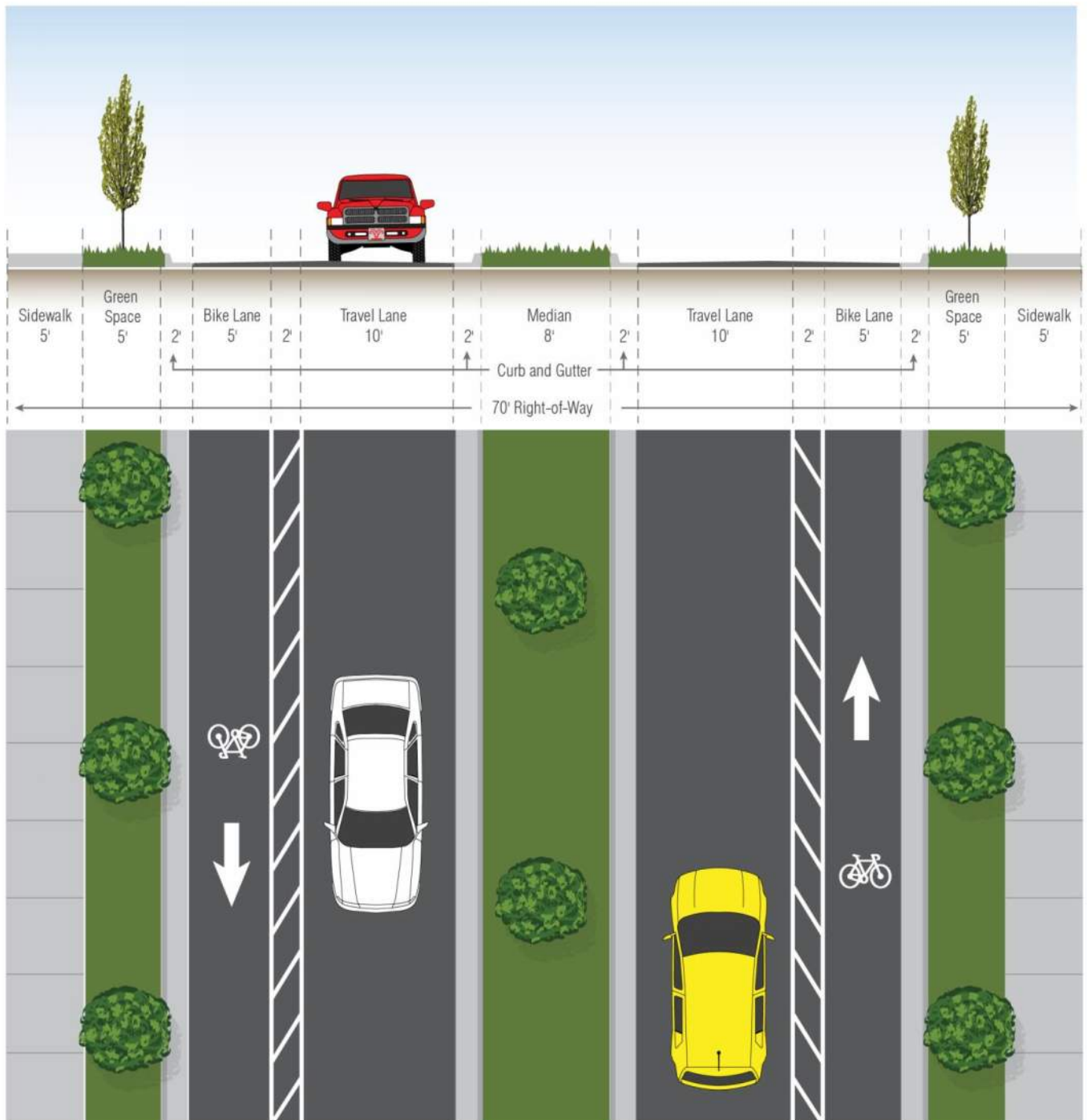




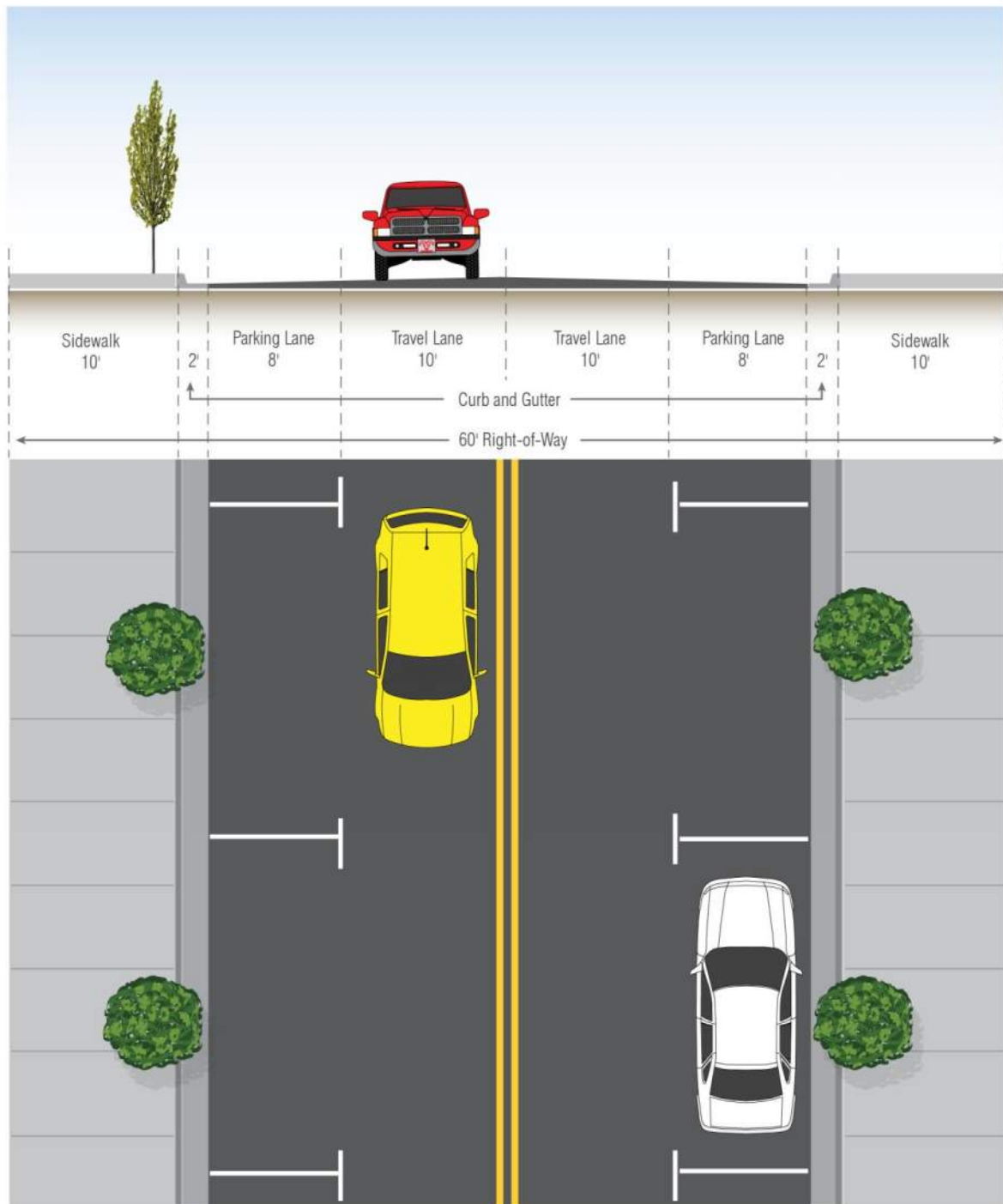
C2.1 – Collector with Median



C2.2II – Collector with Bike Lanes

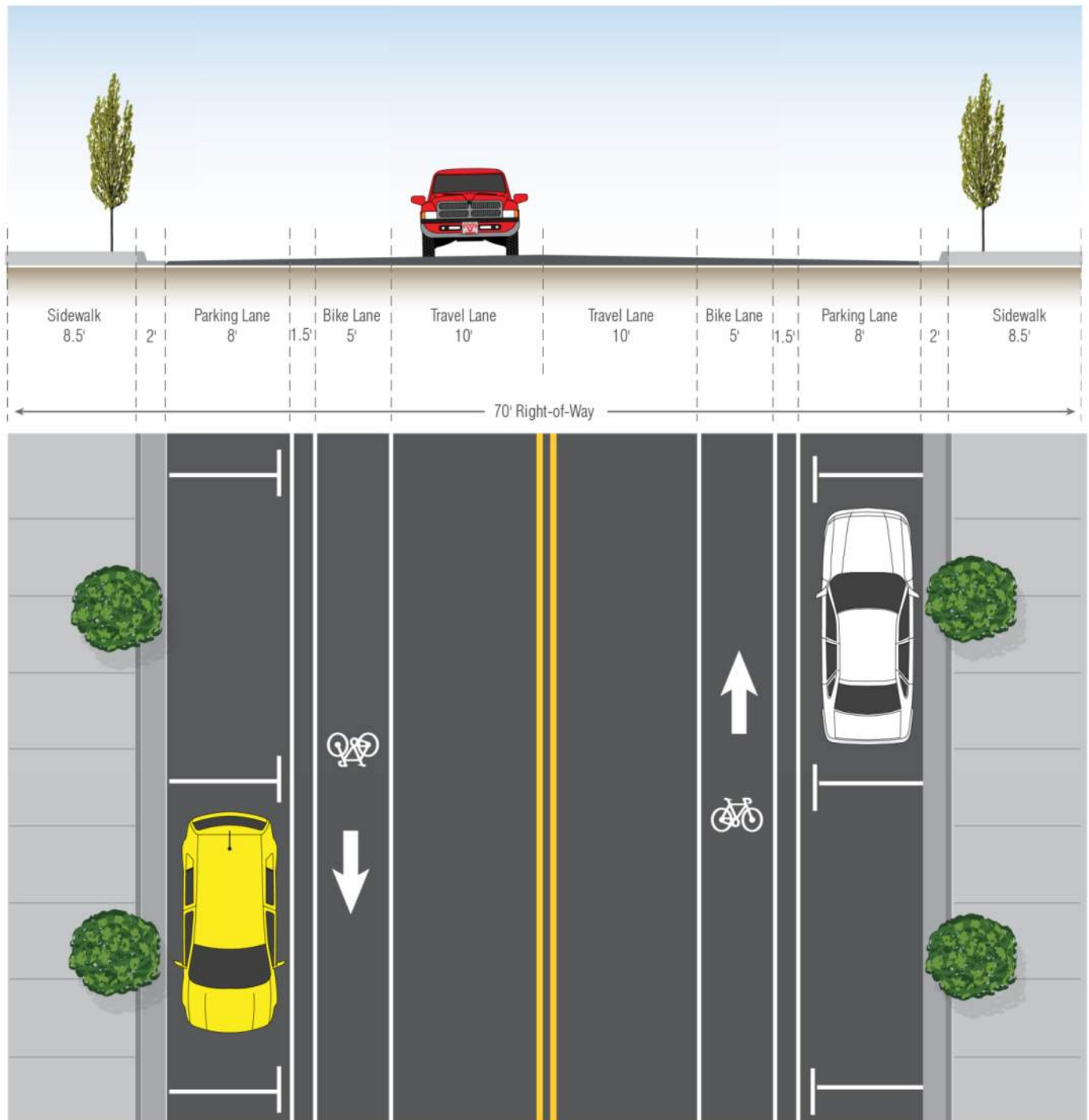


C2.3 – Urban Collector

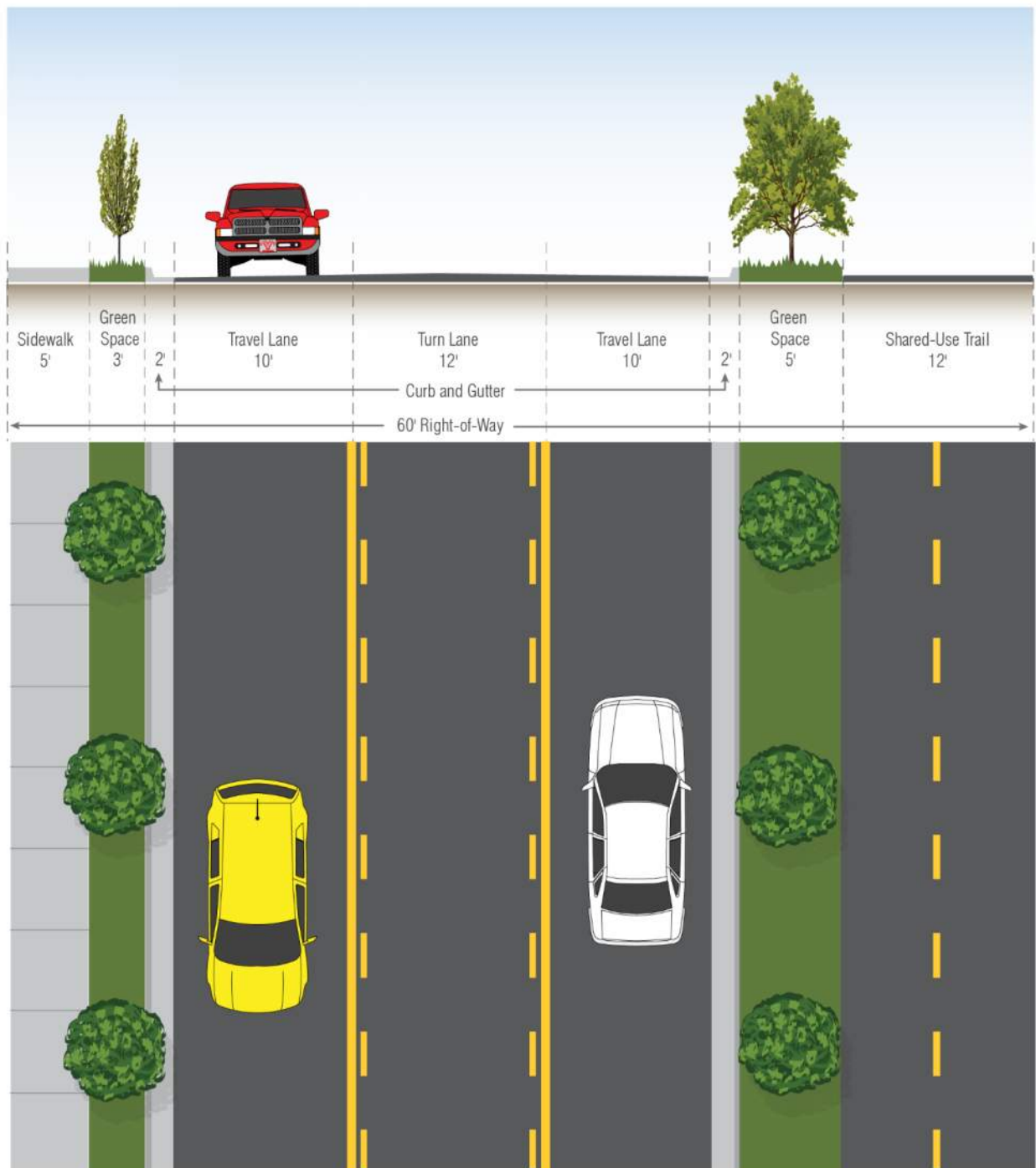


## C2.4II – Urban Collector with Bike Lanes

The location of the parking lanes and bike lanes may be switched to create a protected bike lane if adequate buffer space is provided from the parking lane.

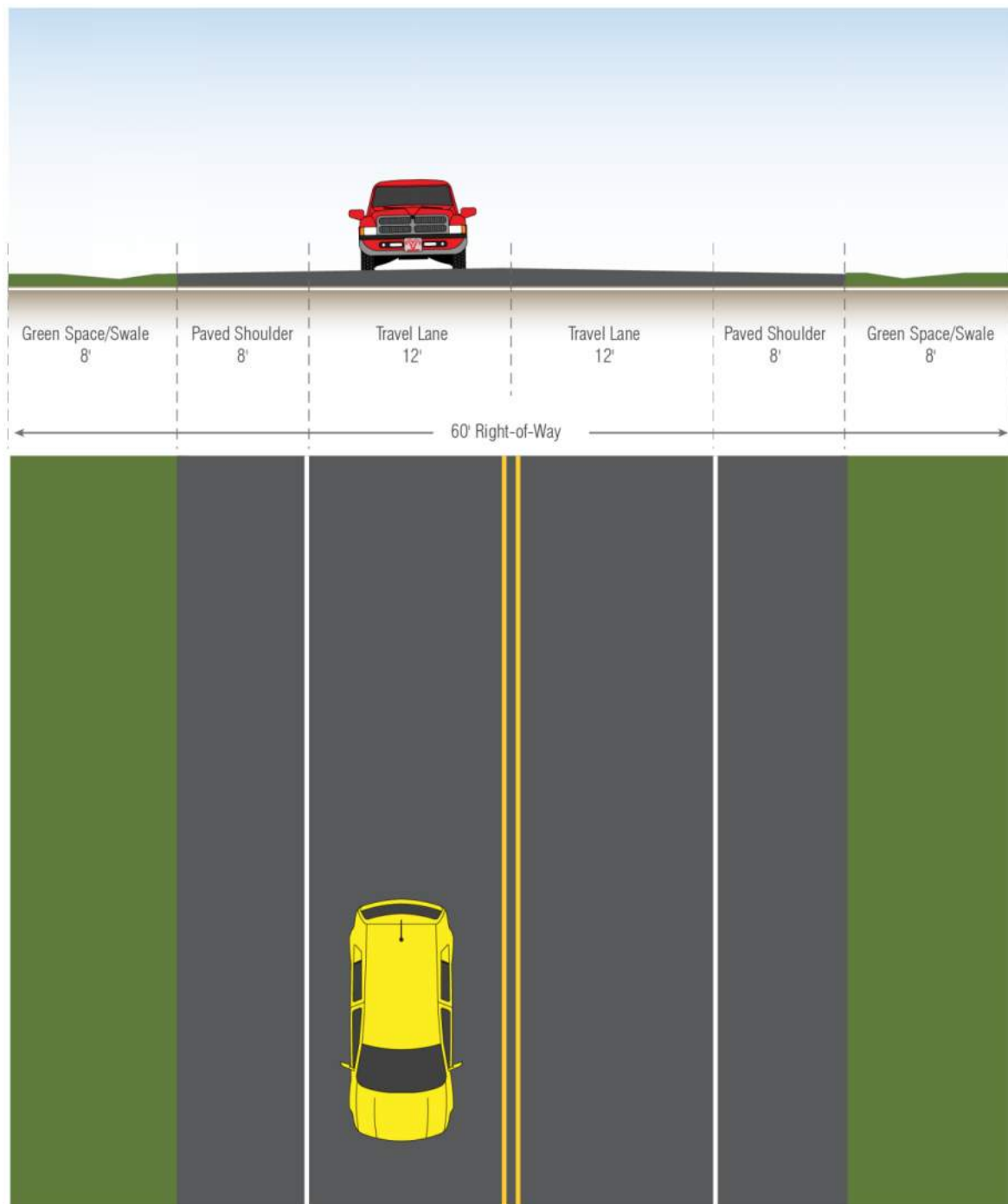


C2.5I – Collector with Trail





C2.6 – Rural/Industrial Collector



### Local Streets

Local streets are intended to connect traffic with collectors and to the arterial roadway network. They are not intended to carry large traffic loads, and are designed for low speeds, providing access to adjoining land.

#### 1. Required Elements:

- a) Right-of-Way: All required design elements must be included in the cross-section and located on publicly owned R.O.W. Sidewalks or bikeways may be located on permanent dedicated easements.
- b) Curb and Gutter: Curb and gutter is required except in cases where terrain and/or forecast land use densities are compatible with an open shoulder design typically used in rural or exurban areas. The gutter width is not to be included in the travel lane.
- c) Sidewalks: Sidewalks are required on both sides of the roadway, except within Industrial Developments (C3.5III). Minimum sidewalk width is 5 ft. and must be compatible with the Americans with Disabilities Act.
- d) Green Space Buffers: A buffer is required between the back of curb and the sidewalk that is a minimum of 3 ft.
- e) Bike Lanes/Trails: If on a planned bikeway route, the bicycle element must be included and must adhere to the bicycle design standards shown on the appropriate cross section.
- f) Lane Width: All lanes shall be used for driving and parking where the combined lanes are greater than 23 ft. in width. Streets on which hydrants are located must have a minimum pavement width of 24 ft. Total street width must be 28' from back of curb to back of curb.

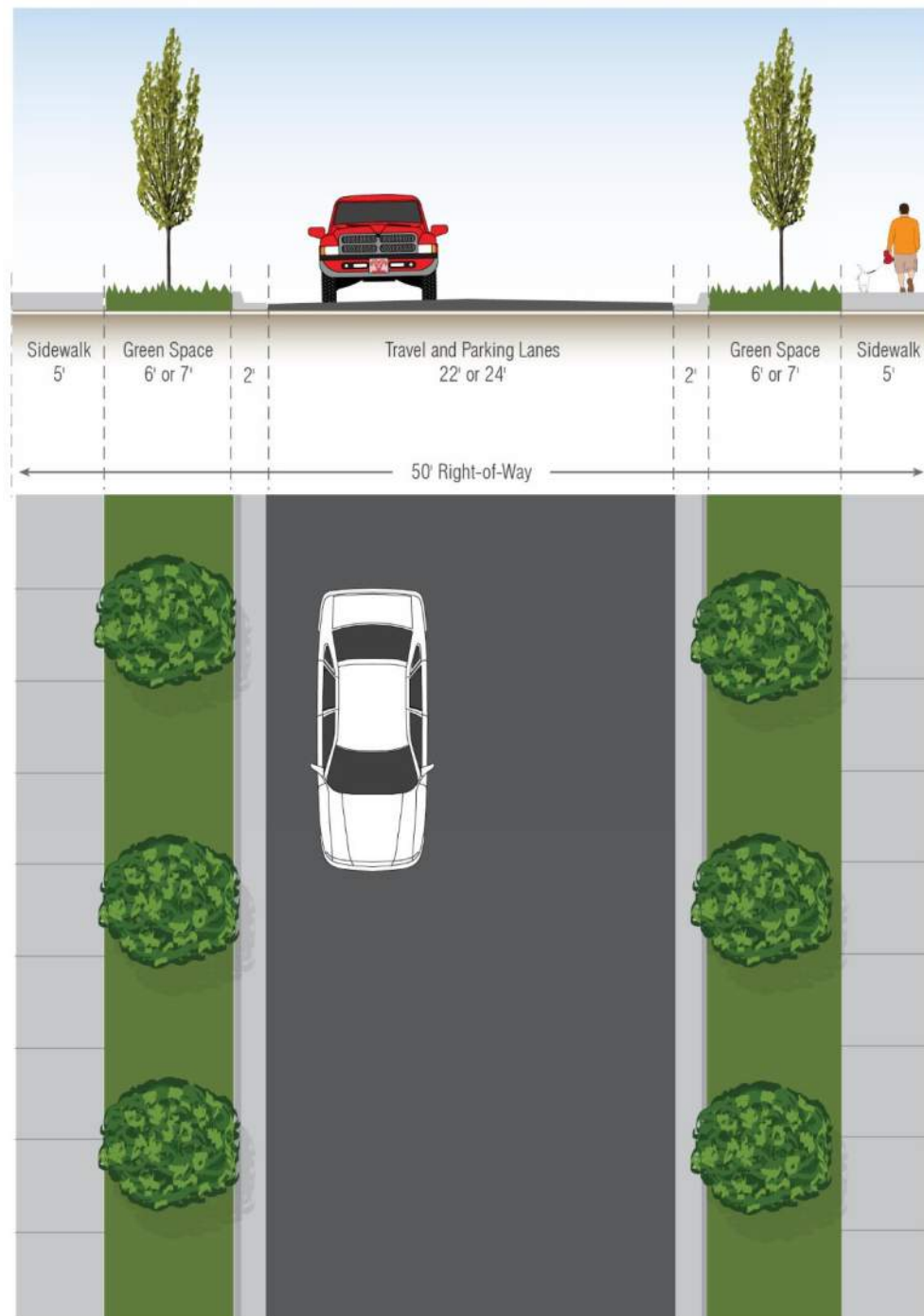
#### 2. Optional Elements:

- a) Parallel parking may be used where warranted.

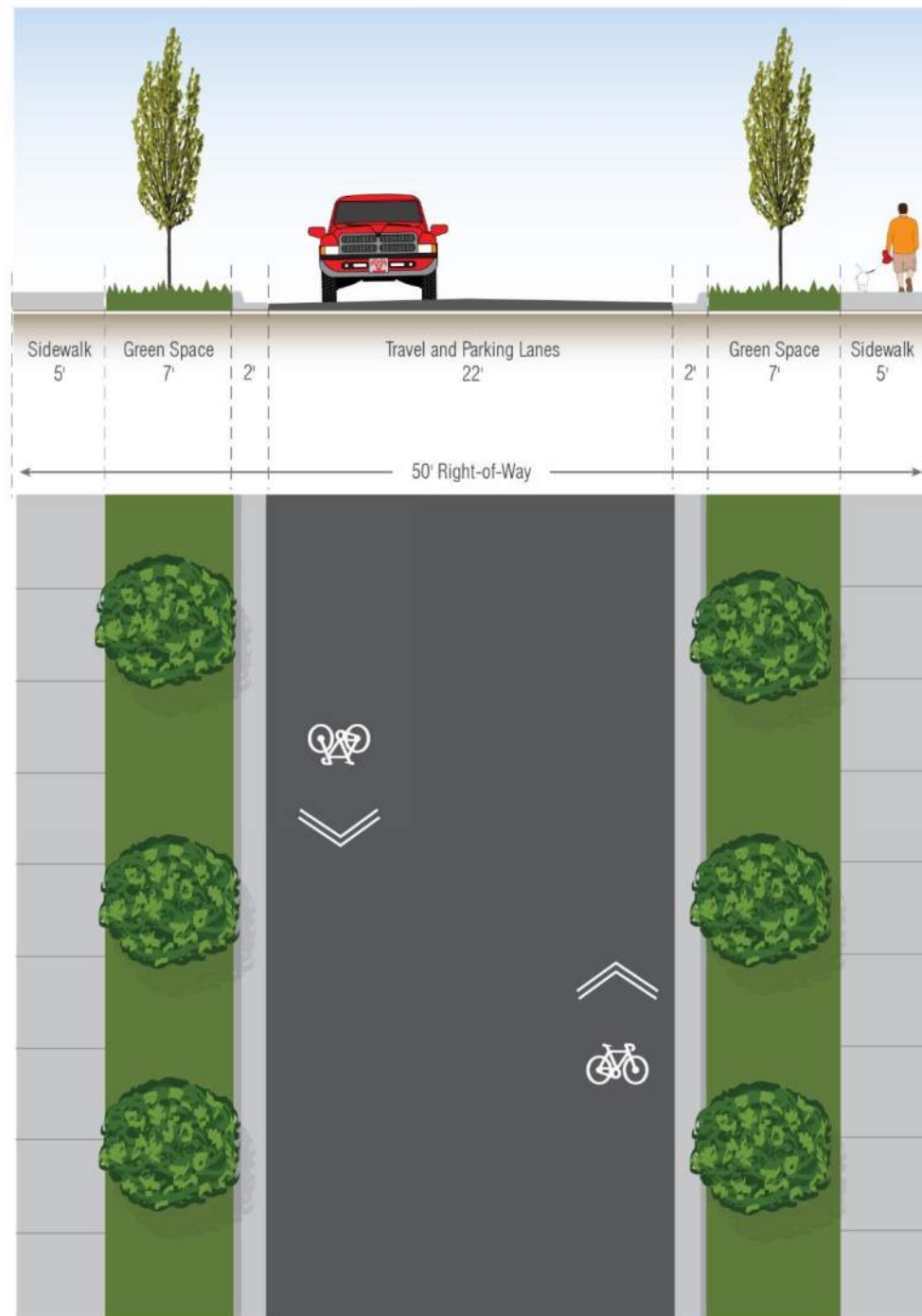
#### 3. Preferred Elements:

- a) Where applicable, a multi-use trail is preferable over bike lanes.

C3.0 – Local Street

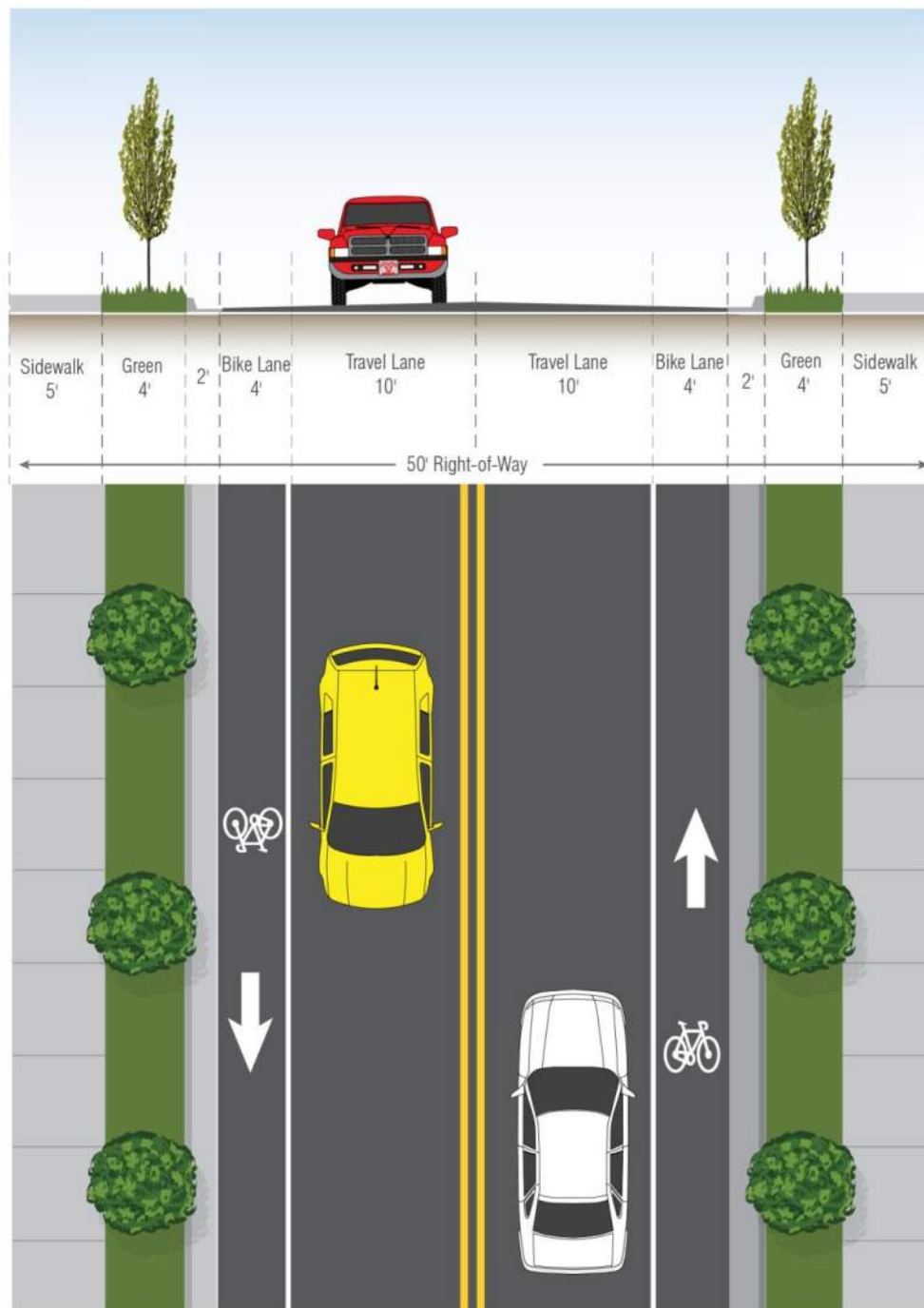


C3.1III – Local Street with Bike Route

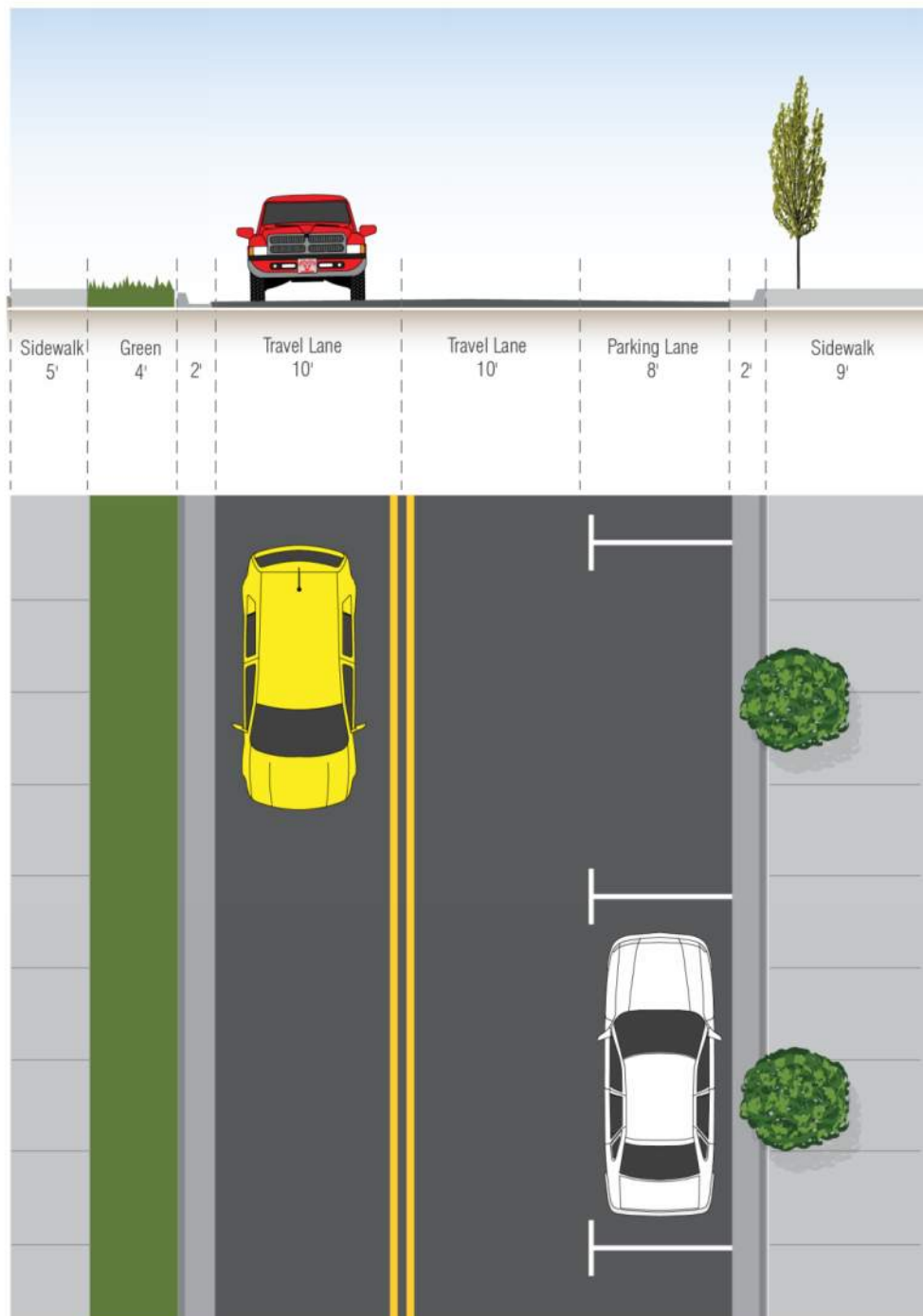




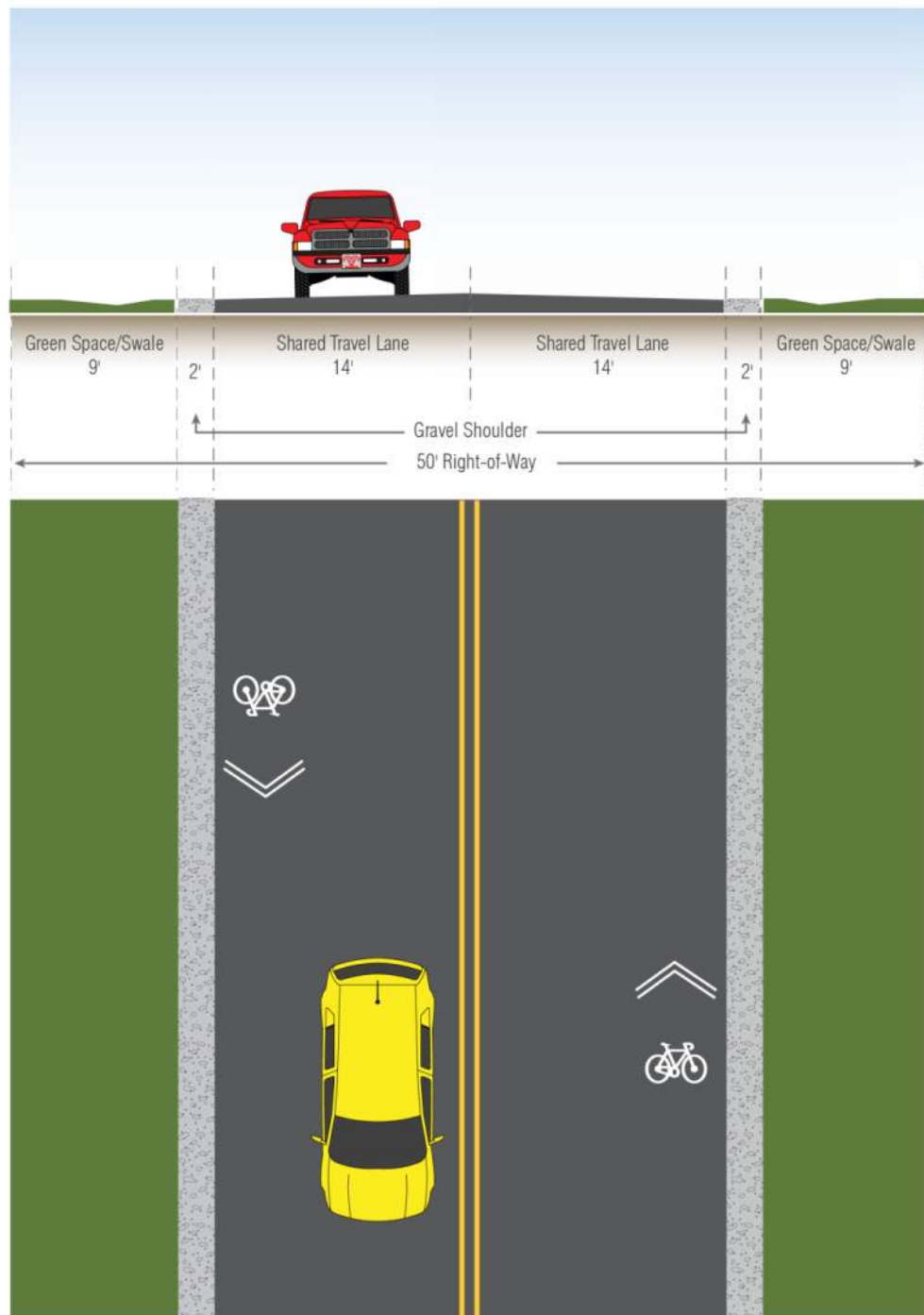
C3.2II – Local Street with Bike Lanes



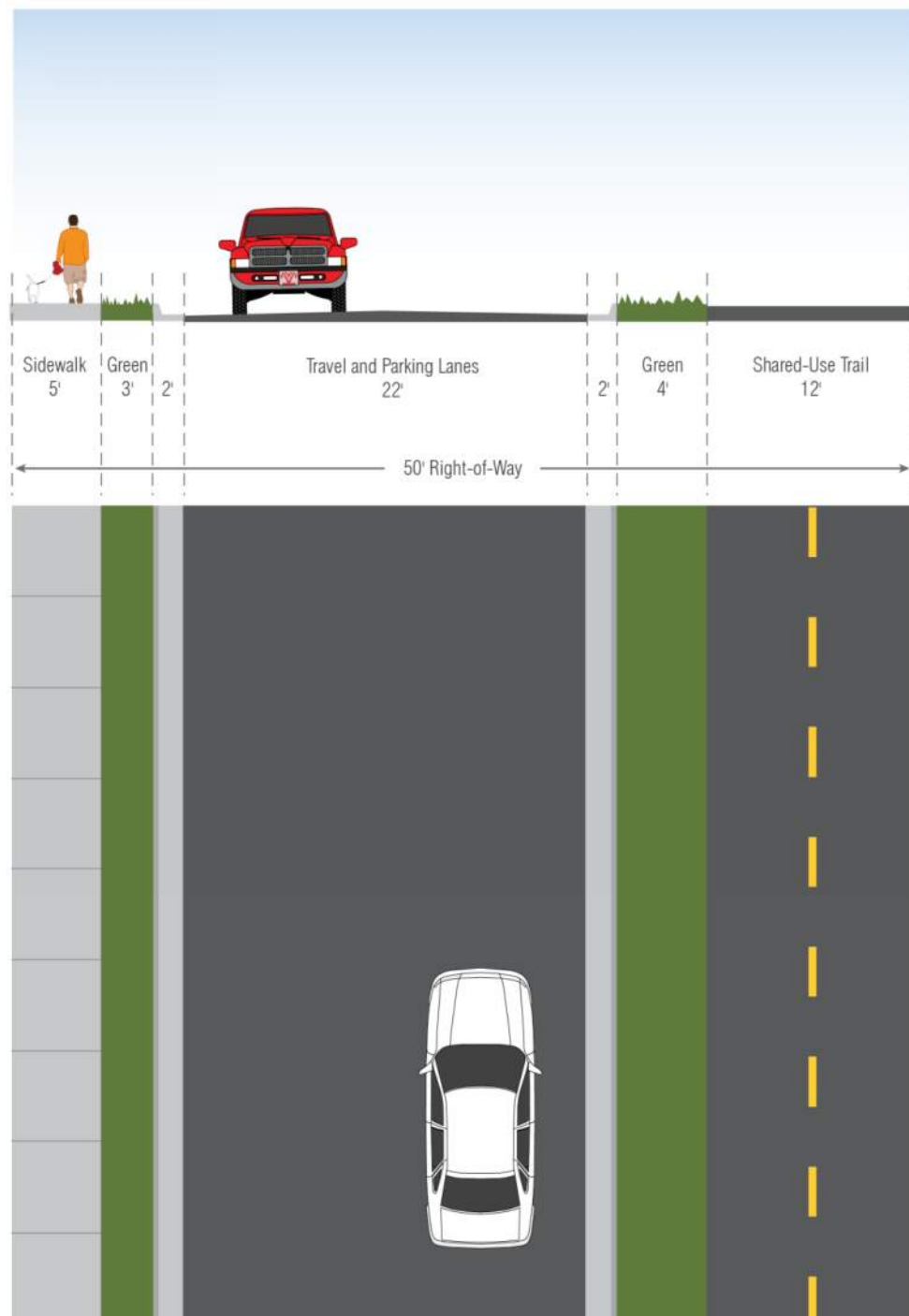
C3.3 – Local Street with Parking Lane



C3.4 – Rural Local Street



C3.5I – Local Street with Trail





### Shared-Use Trails

The following cross section data is to be used only for shared-use trails which are constructed separate from a roadway. For shared-use trails constructed with a roadway see the appropriate cross section above.

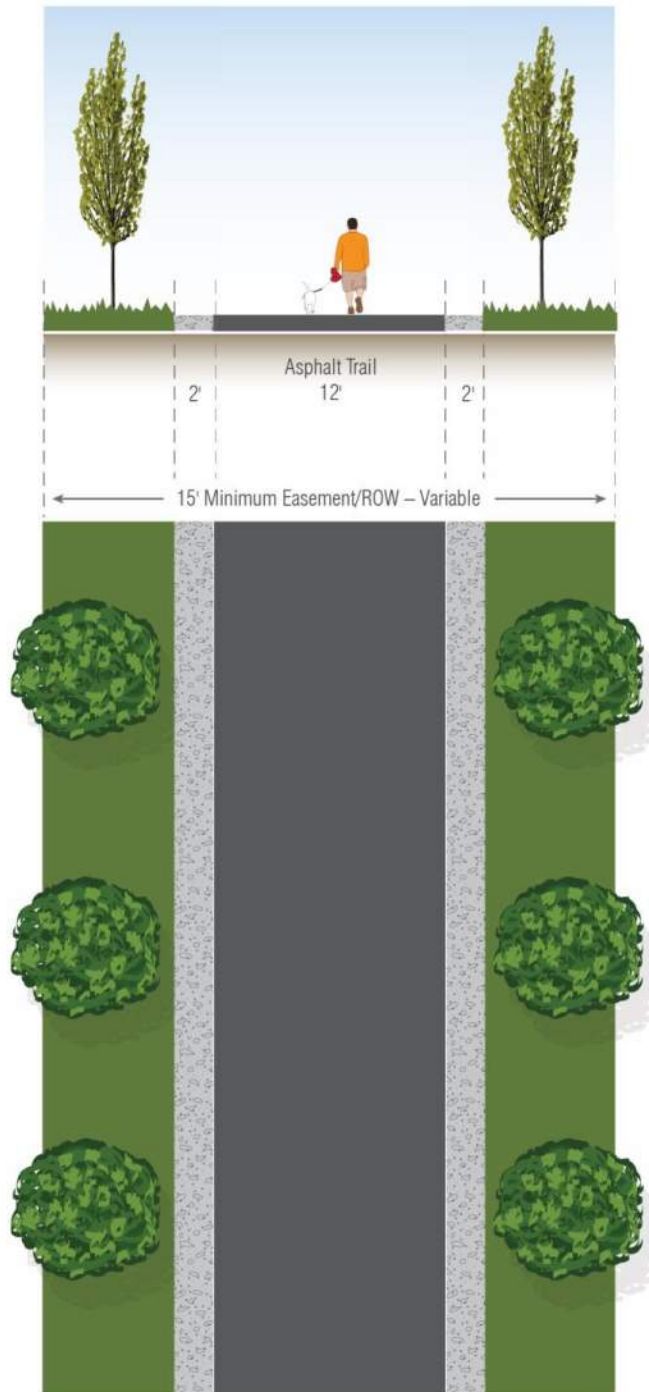
#### 1. Required Elements:

- a) Right-of-Way/Easement: Share-Use Trails may be placed on a dedicated right-of-way or within a permanent dedicated easement. Easement/right-of-way should be adequate for trail and needed amenities. At least 20' is preferable.
- b) Paving Surface: Concrete, Asphalt, or Fine Compacted Aggregate (Decomposed Granite)
- c) Width: Minimum 10' width up to 16' in areas where heavy use warrants a wider trail. 12' Standard
- d) Curb/Shoulder: A suitable shoulder or curbing is required depending on the paving surface. Concrete – None, Asphalt – gravel or concrete, Fine Compacted Aggregate – Concrete or appropriate of edging containment

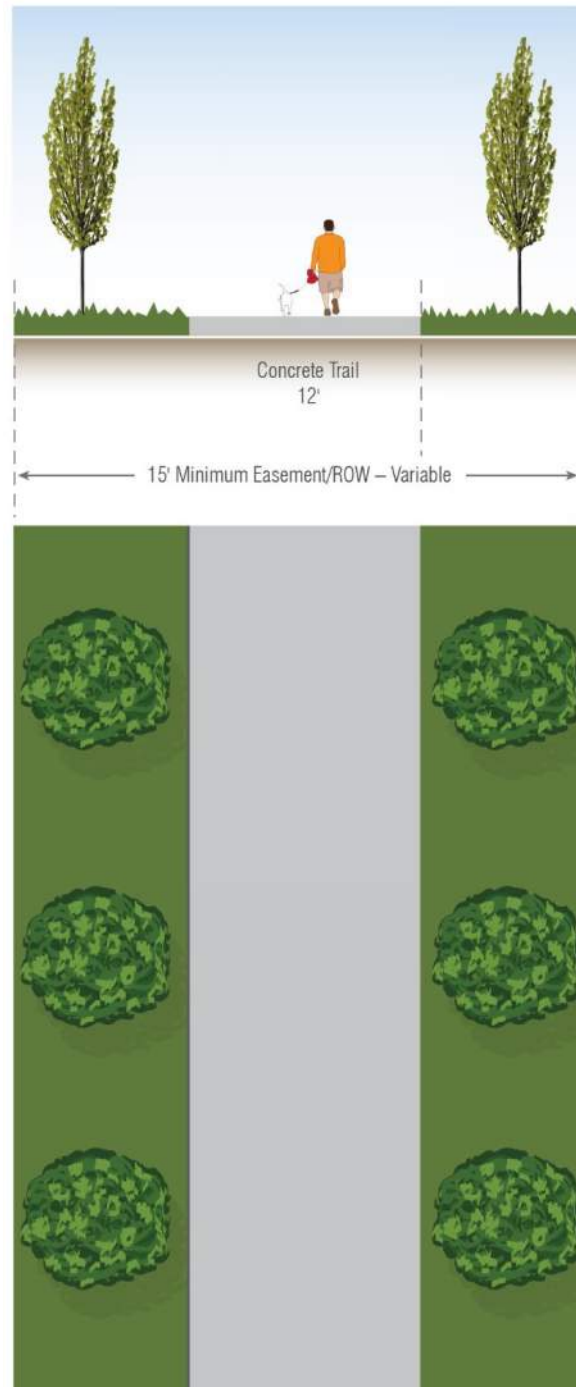
#### 2. Optional/Preferred Amenities:

- a) Park Benches
- b) Public Art Installations
- c) Lighting – where night use is encouraged.

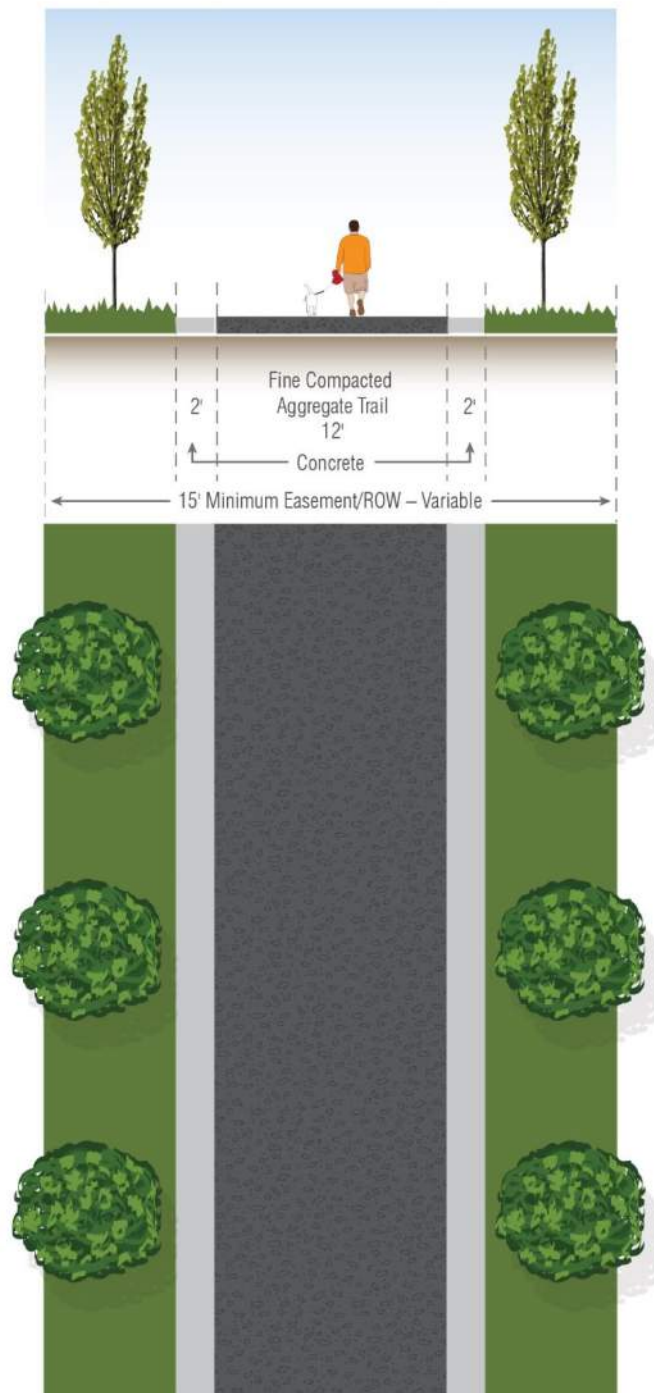
Class I – Shared-Use Trail (Asphalt)



Class I – Shared-Use Trail (Concrete)



Class I – Shared-Use Trail (Aggregate)





## Alternative Cross Sections

The following streets shall be designated as arterials or collectors, but shall be constructed to an alternative cross section based upon traffic studies and intended design plans. The city may designate additional corridors as alternative cross sections without modification of this plan if such alternative cross sections are based upon a publicly funded traffic study or alternative street design.

Alternative Cross Section List		
<i>Street</i>	<i>Street Classification</i>	<i>Design Extents</i>
Country Club Rd.	Minor Arterial	North Hills Blvd. to Beaconsfield Rd.
Maryland Ave.	Minor Arterial	Whitewood Dr. to Stonehill Dr.

## FIVE: IMPLEMENTATION PLAN

### 5.1: Implementation Matrix

GOAL AREA 1. Quality of Life / Recreation Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Develop a connected recreational framework throughout the City					
Invest in parks in areas such as North Sherwood that are currently underserved to improve city-wide parks coverage.	Develop 'Northside' regional park + incorporate 'Kellogg Creek Blueway'			✓	
Connect all parks, schools, and large commercial areas through bike and pedestrian infrastructure to improve accessibility of amenities.	Increase Financial Resources for Trail System Expansion + Implement Master Streets Plan			✓	
Ensure that future street improvements adequately provide for pedestrians, cyclists, and drivers by including sidewalks and trails, where appropriate.					✓
Maintain a Parks Master Plan to guide park development, redevelopment, and programming. This includes periodic updates.	Update Parks Master Plan + incorporate 'Kellogg Creek Blueway'	✓			
Develop both active and passive recreation opportunities and expand recreational programming.			✓		
Pursue partnerships with community institutions to leverage the impact of city recreational facilities.	Update Parks Master Plan + support a Sherwood School District		✓		
Promote the creation of neighborhoods with amenity and a sense of place					
Promote compatible infill development within existing neighborhoods.	Update Development Regulations			✓	
Encourage new neighborhoods that are planned to combine a variety of design elements, uses, densities, housing options, and amenities.			✓		
Provide land use regulations and review processes that promote mixed-use developments.		✓			
Promote residential cluster development that provides common greenspace (small lots with a balance of area reserved for open space).			✓		

GOAL AREA 1. Quality of Life / Recreation Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Promote recreational and cultural programming that enhances quality of life					
Provide quality public space for cultural events that have live music and specialty foods to help bring the community together.	Update Development Regulations		✓		
Encourage ways to include art within the built environment and foster outlets for local performing arts.		✓			
Pursue partnerships with community institutions to provide cultural programming that can have a community-wide benefit.	Update Parks Master Plan		✓		
Encourage community institutions to provide festivals and programming that enhances quality of life.					✓
Support city recreation programs in addition to community recreational programs.				✓	

GOAL AREA 2. Community Identity and Image Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Promote regulations and development that fosters a sense of local identity					
Encourage the preservation of the Old Sherwood neighborhood in the city's southwest section.	Establish 'Historic Sherwood' Overlay District			✓	
Ensure developments provide for sensitive and compatible transitions between residential and nonresidential areas.		✓			
Establish standards for high-quality design of multi-family housing.			✓		
Establish commercial design standards that provide for a unique “Sherwood Character”.		✓			
Pursue events and outreach opportunities that communicate Sherwood's history and identity.					✓
Stimulate redevelopment of Kiehl Avenue and U.S. 67/167 to provide a more positive image of Sherwood.	Establish monument Gateway Signage + Corridor Level Overlay District	✓			
Promote development that provides Sherwood a community focal point and definable city edges					
Promote and encourage the construction of a Town Center style development near the intersection of Brockington Road and Highway 107.	Attract Developer		✓		
Explore ways to enhance the existing city civic complex to create a central community-gathering place and focal point.	Adopt a Civic Center Master Plan		✓		
Adopt regulations to enhance the city's key image gateways such as U.S. 67/167, Brockington Road, Kiehl Avenue, and Highway 107.	Update Development Regulations	✓			
Pursue regulations that encourage high-quality developments that will help better define the borders between Sherwood and North Little Rock.				✓	

GOAL AREA 2. Community Identity and Image Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Use code enforcement as a means to enhance community image					
Use city resources to enforce the city's zoning and subdivision regulations.	Update Staff Memorandums / Policies as Needed				✓
Focus code enforcement efforts on neighborhoods where blighting influences could serve to destabilize property values.		✓			
Use a collaborative approach with property owners in addressing code enforcement violations by helping property owners in need identify resources to assist them.					✓
To use the provisions of Arkansas law such as the "City Cleanup Tools" Act and other laws to address nuisances and unsightly, unsafe, and unsanitary conditions on private property.			✓		
Use the condemnation process as a means to remove abandoned and dilapidated structures that serve to harm the city's neighborhoods.			✓		
Support the creation of an independent Sherwood School District					
Provide support to groups working to create a Sherwood School District.	Update Staff Memorandums / Policies as Needed			✓	
Support those schools that serve Sherwood residents.					✓



GOAL AREA 3. Growth Management and Fiscal Health Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Guide and carefully direct growth in a smart and responsible manner					
Encourage development in areas already served by city services, where service provision is most cost effective.	Develop Annexation Plan that emphasize Benefit / Cost Analysis				✓
Ensure new developments locate where they can be accommodated by existing public infrastructure.	Update Development Regulations				✓
Encourage development that is compatible with the natural and built environment of the surrounding area.					✓
Encourage development that creates long-term community value and adapts well for future re-use.					✓
Promote development that builds the city's tax base and generates sufficient tax revenue to pay for the life-cycle costs of its supporting infrastructure.	Perform Fiscal Study				✓
Create thriving, vibrant neighborhoods, districts, and corridors that are distinct places.	Update Development Regulations		✓		
Promote mixed-use development that combines commercial, residential, and office functions in the same building.				✓	
Adopt, maintain, and use Sherwood Vision 2040 as the city's comprehensive plan					
The planning commission will conduct annual reviews of its comprehensive plan and land use regulations to ensure they remain applicable and up-to-date.	Update Development Regulations	✓			
The city's land use regulations will be consistent with and designed to carry out the provisions of the comprehensive plan.		✓			
Development proposals will be evaluated in terms of their compatibility with the comprehensive plan.					✓
All amendments to the city's land use regulations, including rezoning requests, must conform to the comprehensive plan.					✓

GOAL AREA 3. Growth Management and Fiscal Health Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Enhance the city's land use regulations and development review processes					
Use innovative regulatory tools designed to address community problems identified in the plan such as building design standards, landscaping requirements, character zoning, etc.	Update Development Regulations		✓		
Promote use of land use tools that allow flexibility in site design and layout for innovative developments.		✓			
Support policies that allow the construction of a variety of housing types and price ranges to meet the needs of residents of all ages and incomes.					✓
Provide and carry out land use and building regulations that protect the health, safety, welfare, and aesthetics of the community.	Update Staff Memorandums / Policies as Needed				✓
Provide timely and efficient review of development proposals.					✓
Provide fair, consistent, and transparent review and evaluation of all development proposals.					✓
Ensure development review is conducted in way that evaluates external impacts of development on adjacent properties.					✓
Ensure that the planning commission is representative of the larger community.					✓
Coordinate growth and utilities in the most efficient and effective manner					
Control the extension or provision of utilities in order to carry out the provisions of this plan.	Update Development Regulations			✓	
Ensure no approved development will result in a reduction in the adopted level of service for public infrastructure.			✓		
Require all developments within the Planning Area Boundary to be served by central water and wastewater services, when technically feasible.					✓
Encourage development to occur where it can be supported by the transportation and utility infrastructure.					✓

GOAL AREA 3. Growth Management and Fiscal Health Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
<i>Require all developments to install public utilities and become annexed to the city as a condition of tying onto city utilities.</i>	Develop Annexation Plan that emphasize Benefit / Cost Analysis				✓
<i>Promote development patterns such as mixed-use development that yield higher tax revenue per acre.</i>	Update Development Regulations + Perform Fiscal Study			✓	
<i>Prioritize the maintenance of existing utility and transportation infrastructure over the expansion of new facilities.</i>	Capital Improvement Plan				✓
<i>Ensure local tax policies provide adequate revenue to meet the city's ongoing liabilities.</i>	Perform Fiscal Study				✓
<b>Adequately address issues of drainage and flood hazards</b>					
<i>Ensure developments adequately address drainage to ensure new drainage problems are not created.</i>	Update Staff Memorandums / Policies as Needed				✓
<i>Promote the use of green infrastructure as a way to work with the environment to reduce localized flooding risks and drainage problems.</i>	Update Development Regulations		✓		
<i>Provide regulations that ensure drainage issues caused by development are properly mitigated.</i>					✓
<i>Pursue innovate programs and policies like a stormwater utility to ensure adequate mechanisms are in place to finance public investment in drainage infrastructure.</i>	Explore Stormwater Utility Creation				✓

GOAL AREA 4. Transportation and Infrastructure Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Provide a transportation system that is safe and efficient					
Focus transportation infrastructure investments on corridors that will relieve traffic and improve connectivity.	Consider North Pulaski East-West Connector + Update Master Street Plan (MSP) with proposed road network extensions.				✓
Improve connectivity between North Sherwood and US. 67/167.				✓	
New developments must provide for the interconnection of existing and proposed streets to permit the orderly expansion of the city's transportation system.	Update Development Regulations + Implement connections via existing and proposed collector networks (See with proposed road network extensions in Master Street Plan (MSP) section)				✓
Proposed subdivisions, developments, site plans, or concept plans must comply with this plan. The Planning Commission may consider, on a case by case basis, innovative designs that promote desirable developments without sacrificing the overall goals of this plan, other city plans, or the Arkansas Fire Prevention Code.					✓
New developments adjacent to or encompassing existing streets shall be responsible for construction of half-street improvements to those streets. Those improvements will be consistent with the functional classification reflected in this plan, the city's construction standards, and all other applicable standards.					✓
Where new developments are adjacent to or encompass arterial streets controlled by the Arkansas Department of Transportation (ArDOT), half-street improvements shall meet the functional classification shown on the Master Street Plan or as determined by agreement between the developer, the ArDOT, and the city.					✓
Strip commercial developments are discouraged and may be subject to access limitations. The city encourages commercial developments to provide their own internal streets and drives for direct access to individual outparcels.	Update Staff Memorandums / Policies as Needed		✓		

GOAL AREA 4. Transportation and Infrastructure Policies	ACTION ITEM	Time Frame			
		Short 1-3 yrs.	Medium 3-5 yrs.	Long 5+ yrs.	Ongoing
Provide a transportation system that is equitable and benefits all residents					
Bike and pedestrian facilities will be constructed as part of all new development and transportation facilities according to the provisions of this Plan.	Implement and/or Update Master Street Plan (MSP)				✓
Bike and pedestrian users will be given consideration in the planning and design of all transportation facilities in the planning area.					✓
The city will carefully monitor mobility and access options for citizens with disabilities when reviewing development proposals.					✓
The city will develop a bike and pedestrian transportation system that will consider the mobility and safety needs of a variety of uses including children, seniors, active adults, and the physically challenged.					✓
Utilize context sensitive roadway design approaches to ensure roadways are appropriate for the function of the supporting land use.				✓	
Utilize access management to preserve the function of existing and future corridors					
Access management principles shall be incorporated in the city's land use regulations.	Update Development Regulations + Update MSP Accordingly	✓			
Access points for individual properties fronting collector, minor arterial, and major arterial streets shall be kept to a minimum to facilitate traffic movement, reduce crashes and fatalities, and to increase market areas for local businesses. Keeping access points to a minimum may be achieved through driveway consolidation, joint access agreements, or specific corridor access management plans.					✓
The staff and planning commission shall include considerations of access management principles in the review of all development plans or requests.	Update Staff Memorandums / Policies as Needed				✓



## APPENDICES

### *A.1 Community Engagement Summary*

#### ***Steering Committee Kick-off Meeting Input***

The following contains a summary of the input from the kick-off meeting held with the steering committee for Vision Sherwood 2040. The meeting consisted of moderated discussion focusing on 3 key questions.

#### Question 1 & 2

The participants were asked to “Identify 5 issues/concerns facing Sherwood”. The group then collectively voted to rank the identified issues in order of importance, with the issues of greatest importance receiving the most votes. The issues are listed below with the number of votes received for each issue.

1. Community Development concerns; City hard to work with **(13)**
  - a. NIMBY- Don't build around me
  - b. Need updated Zoning and Land Use Plan
2. Lack of Entertainment, Quality of Life, nothing to do **(6)**
3. Need diverse funding- not just sales tax **(6)**
4. Transportation connectivity and capacity: frustrating intersections, need better car, bike and pedestrian infrastructure **(4)**
  - a. Traffic Flow
  - b. Better Transit Access
5. Lack of Identity **(4)**
  - a. No downtown
  - b. Need to create a Sherwood Experience

#### *Other Issues:*

- Need more passive recreation activities for older population (3)
  - Wetlands; make these an asset
- Need investment in the infrastructure north of town (3)
  - Develop HWY 107/Brockington area of town in a way that creates a long-term asset
- Need more ready to develop sites (2)
- Updated and Competitive schools (2)
- Need more diversity (in race and age) on committees (2)
- Reinvest in infrastructure in the older parts of town (2)
- HWY 67/167 development standards are needed (1)
- Want underground utilities for all future development (1)
- Policies should keep up with the times; outdated City policies (1)
- Community Standards, need more good neighbors
- Wet/Dry issues
- Aging population effects willingness to invest in quality of life
- Older areas that need revitalization, some blight
- More grocery store options needed
- Better maintenance of state HWY needed

Question 3

The group was then asked to identify potential solutions to these concerns or problems. The answers are indicated below.

- Updated Zoning and use codes to bring life/entertainment capabilities to our city.
- Fund and invest in infrastructure
- Plan for structured growth in the northern areas of the City
- Revise/Update General plan
- Update Land Use and Master Street Plans
- Revise zoning, subdivision, and development codes
- Focus on needed street connections and transportation planning
- Develop an identity for Sherwood; Forming an idea of what Sherwood should come to be known for and executing a plan to achieve it
- If warranted- consider a city center plan
- PUD Planning
- Work to bring in entertainment businesses or restaurants
- Slowly work to build a mid/downtown to attract tourists/ younger future citizens
- Redevelop/ clean up existing commercial areas
- Need to learn not to be reactive
- Health: opioid addiction treatment center
- Quality of life; entertainment
- Need fresh blood on community committees and involvement in local politics
- Focus on the best interests for the city overall
- Moderate rate increases on sewer. Extend services to the northern area
- Complete street connections that are planned or imminent
- Passage of bond issues or sales tax
- Acceptance of needed changes
- Repair damaged poles, trees cut down
- Complete parks system
- Transportation Projects (moving traffic from Sherwood into town)
- Continued school improvement

## ***Community Workshop Input***

The following contains a summary of the input from each that participated in the Vision Sherwood Community Workshop. The information is broken down by group.

### *Group 1*

#### *Issues/Recommendations:*

- 1) Wetland trails and camping at old sewer treatment plant and do outdoor education with Clinton Magnet
- 2) Hotel, Restaurant, Professional district at Wildwood and Country Club
- 3) Complete and connect trails across Sherwood
- 4) City Center at Johnson/Hwy 107 or Brockington/Hwy 107
- 5) Canoe/Kayak trails on Kellogg Creek
- 6) Rebuild Hatcher Road Trailer Park
- 7) Rebuild Jacksonville Cato Rd
- 8) Extend I-440 as an arterial road to Hwy 107

#### *Preserve/Enhance/Transform:*

Each group was asked to identify special focal areas within the city. These areas were broken down into three categories:

Preserve: Areas that are currently strong city assets and should be maintain as there currently are.

Enhance: Areas that are currently city assets, but which could be improved upon to achieve greater success.

Transform: Areas which are threats to community stability and must be addressed moving forward.

#### *Preserve:*

- 1) Preserve the Greens at North Hills golf course

#### *Enhance:*

- 1) Historic district for Fairway Avenue area (Old Sherwood)
- 2) Connect trails near City Hall
- 3) Amphitheater north of Sylvan Hills Middle School

#### *Transform:*

- 1) Connect Rock Ridge Dr. to Brockington Rd.
- 2) Redevelop downtown Gravel Ridge
- 3) Fix up the Cherrywood area
- 4) Connect the Maryland Ave. to Maryland Ave. (fill in the gaps)
- 5) Rebuild the Stonehenge Dr. area

### *Group 2*

*Issues/Recommendations:*

- 1) *Connect Rock Ridge Dr. to Brockington Rd.*
- 2) *Connect Hemphill Rd. to Maryland Ave.*
- 3) *Connect Maryland Ave. to Maryland Ave. (fill in the gaps)*
- 4) *Trails along Kellogg Creek*
- 5) *Connect Greens at North Hills to Alanbrook Cv. (trails)*
- 6) *Connect to NLR trails*

*Preserve/Enhance/Transform:*

Each group was asked to identify special focal areas within the city. These areas were broken down into three categories:

Preserve: Areas that are currently strong city assets and should be maintain as there currently are.

Enhance: Areas that are currently city assets, but which could be improved upon to achieve greater success.

Transform: Areas which are threats to community stability and must be addressed moving forward.

*Preserve:*

- 1) *Preserve the Greens at North Hills*

*Enhance:*

- 1) *Grow toward the north areas of the city*
- 2) *Enhance Hwy 107*

*Transform:*

- 1) *Fire access bridge across Indianhead Lake*
- 2) *Need improvements at Lantrip Rd. to behind Kohl's*

### Group 3

#### *Issues/Recommendations:*

- 1) *Need street sign toppers for a) Old Sherwood b) Sherwood proper c) Special Districts*
- 2) *City acquire Lake Cherrywood*
- 3) *Annex to the Northwest*
- 4) *Extend Hemphill Rd., Lantrip Rd., and Mary Lane*
- 5) *Recoup the ARDOT land that was purchased for the Northbelt*
- 6) *Allow motorsports along Kellogg Creek*

#### *Preserve/Enhance/Transform:*

Each group was asked to identify special focal areas within the city. These areas were broken down into three categories:

Preserve: Areas that are currently strong city assets and should be maintain as there currently are.

Enhance: Areas that are currently city assets, but which could be improved upon to achieve greater success.

Transform: Areas which are threats to community stability and must be addressed moving forward.

#### *Preserve:*

- 1) *Preserve Greens at North Hills*
- 2) *Preserve Sherwood Forest*

#### *Enhance:*

- 1) *Enhance Brockington/Hwy 107 with a Town Center*
- 2) *Create a Wildlife area along Kellogg Creek*
- 3) *Enhance old Sherwood as an historic district*
- 4) *Enhance the Trammel Road area*

#### *Transform:*

- 1) *Improve the Cherrywood area and have city acquire Lake Cherrywood*
- 2) *Cleanup area between Summit St. and Jamesway Dr.*
- 3) *Annex to the Northwest*



Group 4

*Issues/Recommendations:*

- 1) *Need municipal broadband*
- 2) *Need more entertainment options (restaurants, movies, etc.)*
- 3) *Bring appeal to Sherwood (indoor entertainment)*
- 4) *Fix Zoning Issues (clear up misconceptions)*
- 5) *Trader Joes/Event Center at Gander Mountain*
- 6) *Add Dog Park at Koehler/Fairway Ave.*
- 7) *Target Chik-fil-a, Starbucks, Trampoline Park, Bowling Center*
- 8) *Brockington Rd. has entertainment potential*

*Preserve/Enhance/Transform:*

Each group was asked to identify special focal areas within the city. These areas were broken down into three categories:

Preserve: Areas that are currently strong city assets and should be maintain as there currently are.

Enhance: Areas that are currently city assets, but which could be improved upon to achieve greater success.

Transform: Areas which are threats to community stability and must be addressed moving forward.

*Preserve:*

- 1) *Preserve "Old" Sherwood near Sherwood Ave/Fairway Ave.*

*Enhance:*

- 1) *Fix Holiday and Lee Intersection*
- 2) *Fix up the Indiandale area*
- 3) *Enhance Brockington/107 with Grocery and Town Square*
- 4) *Enhance visual appeal of Hwy 107 north of Jacksonville Cutoff Rd.*

*Transform:*

- 1) *Flood control of Kellogg Creek*
- 2) *Improve Jacksonville Cutoff Rd.*

Group 5

*Issues/Recommendations:*

- 1) *Need high standards for development*
- 2) *Mixed use would be good if well done*
- 3) *Require landscaping/better signage*
- 4) *Consider a form-based code*
- 5) *Freeze problematic development in Gravel Ridge*
- 6) *Build an Art Trail in Sherwood Forest*
- 7) *Use old sewer treatment plan to build a new park*
- 8) *Create a blueway canoe trail on Kellogg Creek and Bayou Meto*
- 9) *Build bike lanes and trails everywhere and connect to NLR*
- 10) *Consider retaining street easement though Camp Robinson for an arterial connection to I-430*

*Preserve/Enhance/Transform:*

Each group was asked to identify special focal areas within the city. These areas were broken down into three categories:

Preserve: Areas that are currently strong city assets and should be maintain as there currently are.

Enhance: Areas that are currently city assets, but which could be improved upon to achieve greater success.

Transform: Areas which are threats to community stability and must be addressed moving forward.

*Preserve:*

- 1) *Preserve Kellogg Creek*
- 2) *Preserve Narragansett Place crossing across Indianhead Lake*
- 3) *Preserve greenspace at the Greens at North Hills*

*Enhance:*

- 1) *Take more advantage of Sherwood Forest*
- 2) *Build a town square at City Hall*
- 3) *Create wetland park between Hollywood Ave and Staff west of Clinton Elem.*
- 4) *Enhance the Greens at North Hills (switch to 9-hole, open park on other 9)*
- 5) *Improve the appearance of the Brockington Rd. flyover from US 67/167*

*Transform:*

- 1) *Fix Kiehl and Brockington intersection*
- 2) *Improve capacity of Hwy 107 and Kiehl Ave.*
- 3) *Fix Country Club/North Hills/Hwy 107*
- 4) *Improve roadway between North Hills and Kiehl Ave.*
- 5) *Fix up Cherrywood area*
- 6) *Code enforcement for area near Brookwood Village*
- 7) *Improve appearance of Hwy 107 in Gravel Ridge*

## ***Visual Preferences***

At the Vision Sherwood community workshop, participants rated images of development on a scale of 1 through 5, with 1 being least desirable and 5 being most desirable. The purpose was to identify compatible forms of development.

Infill Duplex



*Average: 3.08*

Standard Duplex



*Average Score: 1.92*

Apartments with Landscaping



*Average Score: 2.96*

Single-Family w/ Protruding Garage



*Average Score: 3.00*

Apartment Complex w/ Amenities



*Average Score: 3.87*

Mansion Style Apartment



*Average Score: 2.84*



Conventional Single-Family



*Average Score: 3.24*

Metal Single-Family



*Average Score: 1.24*

Small Setback Single-Family



*Average Score: 4.13*

Townhomes



*Average Score: 3.72*

New Urbanist Single-Family



*Average Score: 3.92*

Small-Scale Multi-Family



*Average Score: 2.18*

Improved Manufactured Home



*Average Score: 2.92*

Rural Single-Family



*Average Score: 3.68*

Mixed-Use Development



Average Score: 4.29

Gas Station



Average Score: 3.13

High Quality Commercial Center



Average Score: 4.39

Conventional Big Box Store



Average Score: 2.83

Conventional Strip Center



Average Score: 2.17

Walkable Mixed-Use Center



Average Score: 4.57